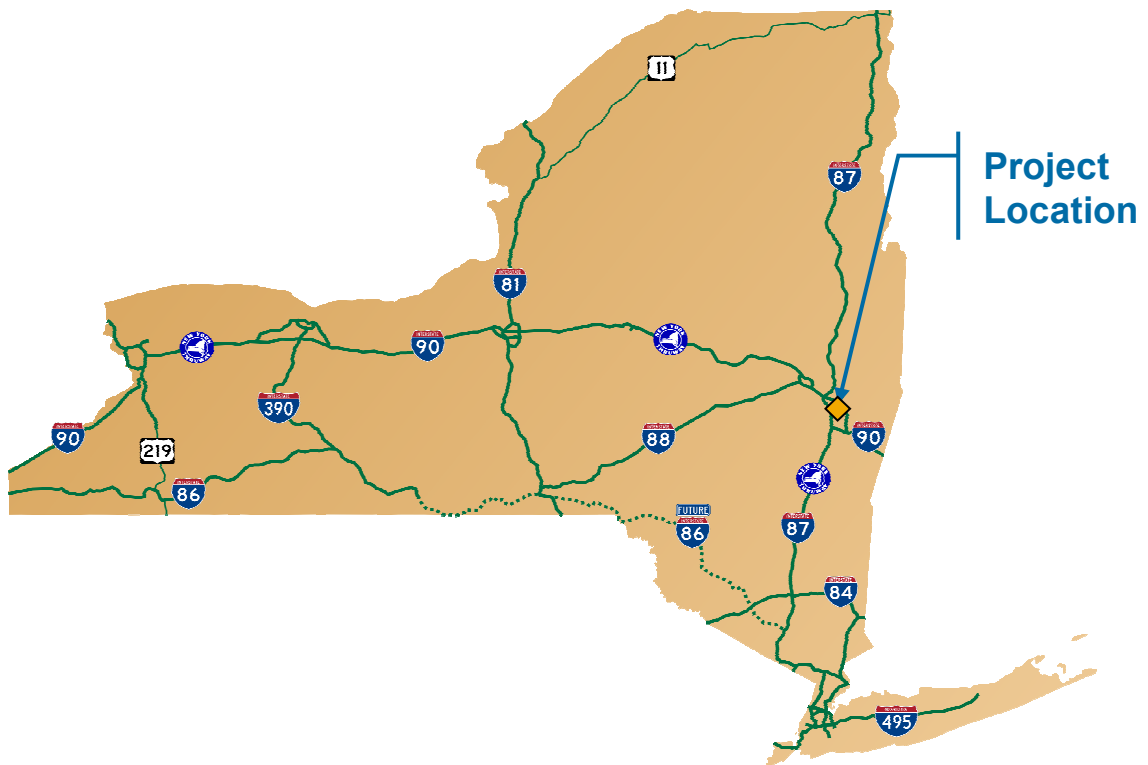


Transportation Project Report

Draft Design Report

June 2019

Pedestrian Safety Improvements at Various Locations
Project Identification Number (PIN): 1760.94
City of Rensselaer
Rensselaer County



Department of
Transportation



Project Approval Sheet

<u>Milestones</u>	<u>Signatures</u>	<u>Dates</u>
A. Recommendation for, Initiation, Scope and Design Approval:	The project cost and schedule are consistent with the Regional Capital Program. _____ Regional Program Manager	_____ Date
B. Recommendation for Scope and Design Approval	No nonstandard features have been identified, created, or retained. _____ Greenman-Pedersen, Inc	_____ Date
C. Public Hearing Certification (23 USC 128):	A public hearing was not required. _____ Greenman-Pedersen, Inc	_____ Date
D. Categorical Exclusion Determination on Behalf of FHWA	This project qualifies as a Categorical Exclusion under the National Environmental Policy Act per the NYSDOT/FHWA Programmatic Agreement Regarding Categorical Exclusions. _____ NYSDOT Regional Director	_____ Date
E. Local Project Nonstandard Feature Approval	No nonstandard features are being retained or created on Non-NHS local roadways. _____ Responsible Local Official	_____ Date
F. Local Project Scope and Design Approval	The required environmental determinations have been made, and the preferred alternative for this project is ready for final design. _____ Responsible Local Official	_____ Date

CONTACT: Charles Moore, Director of Planning and Development

City Hall, 62 Washington Street

Rensselaer, NY 12144

PHONE: (518) 465-1693

List of Preparers

Group Director Responsible for Production of this Initial Project Proposal/Final Design Report (IPP/FDR):

Reviewed By: Mike Wieszchowski, P.E., PTOE
Assistant Vice President, Greenman-Pedersen, Inc.

Description of Work Performed: Directed the preparation of the IPP/FDR in accordance with established standards, policies, regulations and procedures, except as otherwise explained in this document.

PLACE P.E. STAMP

Note: *It is a violation of law for any person, unless they are acting under the direction of a licensed professional engineer, architect, landscape architect, or land surveyor, to alter an item in any way. If an item bearing the stamp of a licensed professional is altered, the altering engineer, architect, landscape architect, or land surveyor shall stamp the document and include the notation "altered by" followed by their signature, the date of such alteration, and a specific description of the alteration.*

1.1 PUBLIC FRIENDLY DESCRIPTION OF PROJECT

NYSDOT has established a Pedestrian Safety Action Plan (PSAP), which is a five-year multi-agency plan that takes a three-pronged approach to improve pedestrian safety. The plan includes focusing on engineering improvements to improve pedestrian safety and minimize crash potential by implementing countermeasures at the crossings on local urban roads and streets. Projects funded through the PSAP are funded through the federal Highway Safety Improvement Program.

The City of Rensselaer submitted a PSAP application form with supporting documentation and the following signalized intersections were selected for funding:

1. Intersection of Washington Avenue, Forbes Avenue and 7th Street
2. Intersection of Broadway and Partition Street
3. Intersection of Broadway and Pine Street
4. Intersection of Broadway and McNaughton Avenue
5. Intersection of 1st Street, Partition Street and East Street
6. Intersection of 3rd Street and Catherine Street.

A project location map is shown on the cover sheet of the project plans attached in Appendix A.

1.2. PROJECT LOCATION

- A. City/Village/Township: City of Rensselaer
- B. County: Rensselaer County
- C. Any other description information which is pertinent: The project is located at the six intersections listed in section 1.1
- D. Length: 6 Intersections totaling approximately 430± linear feet.
- E. Federal Aid System: **Highway Safety Improvement Program** FUNCTIONAL CLASS: **Varies (see section 2.1)**
- F. Existing AADT: **See critical design elements in Section 2.1.**
- G. Trucks (%): **See critical design elements in Section 2.1.**

1.3. PROJECT NEEDS

Existing Characteristics of Concern	
Element	Measure/Indicator
Traffic Signal Deficiencies	All 6 intersections lack pedestrian push buttons except the corner of Washington and Seventh
	The intersection of Broadway and Pine had a flashing yellow light.

Curb Ramp/Pedestrian Facility Deficiencies	<p><u>Washington Avenue, Forbes Avenue and 7th Street –</u> The intersection lacks pedestrian signals at 3/4 of the quadrants of the intersection. 2 of the crosswalks aren't high intensity ladder style. The ramps at the south side of Washington and 7th, and Washington and Forbes lack detectable warning units. The existing ramp at the north end of Washington and Forbes has a slope that is not ADA compliant.</p> <p><u>Broadway and Partition Street –</u> The intersection lacks pedestrian push buttons and signals at all quadrants of the intersection. The existing crosswalks are not high intensity ladder style. The existing curb ramps are not ADA compliant.</p> <p><u>Broadway and Pine Street –</u> The intersection lacks pedestrian push buttons and signals at all quadrants of the intersection. The intersection lacks crosswalks. The existing traffic signal is only one flashing yellow light.</p> <p><u>Broadway and McNaughton Avenue –</u> The intersection lacks Pedestrian push buttons and signals at all quadrants of the intersection. The existing curb ramps lack detectable warning units. There are no crosswalks. There is only one traffic head in each direction.</p> <p><u>1st Street, Partition Street and East Street –</u> The intersection lacks Pedestrian push buttons and signals at all quadrants of the intersection. The existing curbs ramps at the east corner of partition and east street, the east facing curb ramp at the west corner of partition and east street lack detectable warning units and are not ADA compliant. The crosswalks are also all not high intensity ladder style.</p> <p><u>3rd Street and Catherine Street –</u> The pedestrian push buttons do not work. The intersection lacks pedestrian signals. The intersection lacks crosswalks. There are no detectable warning units at the southwest corner of the intersection.</p>
Other Pertinent Measure(s)	N/A

Project Element(S) To Be Addressed:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Highway Element-Specific | <input checked="" type="checkbox"/> Operational Maintenance |
| <input type="checkbox"/> Bridge Element-Specific | <input type="checkbox"/> Where & When |
| <input type="checkbox"/> Other: | |

Priority Results:

- | | | |
|---|--|-----------------------------------|
| <input type="checkbox"/> Mobility & Reliability | <input checked="" type="checkbox"/> Safety | <input type="checkbox"/> Security |
| <input type="checkbox"/> Economic Competitiveness | <input type="checkbox"/> Environmental Stewardship | |

1.4. PURPOSE/OBJECTIVES

- (1) Correct safety deficiencies using cost effective accident reduction measures such that accident reduction benefits equal or exceed project costs attributable to safety work.

1.5. DESCRIPTION OF PROPOSED WORK

The proposed work for this project will include installation of pedestrian signals, curb ramps, and striping all of which are described below.

Curb Ramps:

The proposed impacts to the existing curb ramps and sidewalk will vary per intersection. Prior to construction, all existing curb ramps will be inventoried and evaluated based on the critical design elements for pedestrian facilities. Those not meeting current design standards will be replaced and installed with detectable warnings. Similarly, pavement conditions at crosswalk locations will be reviewed, and crosswalk areas will be milled and resurfaced if pavement is deemed unacceptable.

Pedestrian Signals:

With the exception of Washington Avenue and 7th street, the other five intersections lack any pedestrian signals. As part of the project, pedestrian signals will be constructed to provide safe passage across both the major and minor approaches of each intersection. A preliminary survey of the existing traffic signal controllers indicates that the controller at Broadway and McNaughton Avenue will need to be replaced so that it can accommodate the addition of pedestrian timings. All other controllers will remain. To minimize costs and impacts to roadway the project will attempt to re-use conduit wherever feasible. All new pedestrian signals installed will include Accessible Pedestrian Signals (APS) pushbuttons.

Striping /Signage:

At the six signalized intersections, the existing stop bars and crosswalks will be removed and replaced. The crosswalks will be upgraded to Type ladder-standard (LS) to increase visibility. Signage will also be upgraded and installed to ensure compliance with the MUTCD and latest NYSDOT Guidance. Additionally, all school sign posts will be installed with two sign posts and reflective strips per TSMI 16-03 for increased visibility. Below is a summary of the planned improvements:

Traffic signals:

This project is expected to have a minor impact on the existing traffic signals. At the intersections of Broadway/Pine Street and Broadway/McNaughton Avenue a microwave detector will be added to serve the minor approach thereby converting each intersection to semi-actuated operation. Each intersection currently operates under a pretimed condition which is inefficient for motor vehicles, but also fails to allow the integration of a pedestrian phase. Under a pretimed condition pedestrian phase would activate under each cycle which renders the use of APS pushbuttons unnecessary. Outside of the microwave detectors and replacement of the outdated controller at Broadway and McNaughton Avenue mentioned above, the project will not adjust field timings (for lane optimization), head layout, or detection.

For a more in-depth discussion of the design criteria and nonstandard features **see section 2.1** of this report.

2.1 DESIGN STANDARDS

Design Standards	
Project Type	NYSDOT Design Guidance
Sign and/or Traffic Signal Upgrading Projects	NYSDOT Highway Design Manual Chapter 11
Bicycle and Pedestrian Facilities	NYSDOT Highway Design Manual Chapters 17 & 18
Drainage System Restoration	NYSDOT Highway Design Manual Chapters 8 & 19
1R Projects	NYSDOT Highway Design Manual Chapter 7
Engineering Directive 15-004	Design, Construction and Inspection of Pedestrian Facilities in the Public Right of Way
Traffic Safety and Mobility Instruction 17-07	Pedestrian Safety Action Plan (PSAP) Countermeasure Details

2.2 OTHER DESIGN PARAMETERS

There are no other design parameters.

2.3 NON-STANDARD/NON-CONFORMING FEATURES -

There are no nonstandard or nonconforming roadway features within the project limits.

Existing pedestrian facilities within the scope of this project will be evaluated in final design for conformance with the applicable standards in the NYSDOT Critical Elements for the Design, Layout and Acceptance of Pedestrian Facilities found on the NYSDOT Highway Design Manual [Chapter 18 webpage](#). If the work at any facility will not meet the applicable standards, then the procedural requirements identified in ED 15-004 - Design, Construction and Inspection of Pedestrian Facilities in the Public Right of Way will be followed and the facility will be rehabilitated, replaced or justified as nonstandard.

2.4 SPECIAL TECHNICAL ACTIVITIES REQUIRED

2.5 WORKZONE SAFETY & MOBILITY

The City has determined that this project is not significant per 23 CFR 630.1010.

A Transportation Management Plan (TMP) will be prepared for the project consistent with 23 CFR 630.1012. The TMP will consist of a Temporary Traffic Control (TTC) plan. Transportation Operations (TO) and Public Information (PI) components of a TMP will be considered during final design.

2.6 ASSET MANAGEMENT (OPTIONAL)

☐ Applies ☒ Not Applicable

2.7 POTENTIAL UTILITY INVOLVEMENT

☐ Yes ☒ No

2.8 RIGHT OF WAY

All proposed work can be accomplished within the existing right-of-way, therefore, it is anticipated that no right of way acquisitions will be required for the project. The ROW Clearance Certificate will be attached to the PS&E transmittal memo.

3.1 ENVIRONMENTAL CLASSIFICATION

NEPA (National Environmental Policy Act):

In accordance with the Federal Highway Administration's regulations in 23 CFR 771.117(c) this is an action which will not have significant environmental effects and does not normally require additional federal approval regarding NEPA. Specifically this action meets the description in 23 CFR 771.117(c)(8) described as "Installation of fencing, signs, pavement markings, small passenger shelters, traffic signals, and railroad warning devices where no substantial land acquisition or traffic disruption will occur." and meets the constraints listed in 23 CFR 771.117(e). This is further detailed in the Federal Environmental Approvals Worksheet (FEAW) included in Appendix B.

SEQRA (State Environmental Quality Review Act):

This project is being processed as a SEQR Type II Action in accordance with 6 NYCRR Part 617, in which the City of Rensselaer will be the lead agency.

The following Checklist(s) are attached:

☒ Federal Environmental Approvals Worksheet (FEAW)

☒ Social, Economic and Environmental Resources Checklist

☒ Capital Projects Complete Streets Checklist

3.2 ENVIRONMENTAL DOCUMENTATION

Comparison of Alternatives		
Category	Alternatives Evaluated	
	Null	Reasonable/Preferred Alternative – Alt. 1
Social, Economic and Environmental Resources Checklist (SEERC)		
Social - B. - Neighborhoods and Community Cohesion - #3	None	The project will provide improvements to walking and bicycle options to nearby business and residential areas
Social - C. - General Social Groups - #1	None	The project will improve existing conditions and provide safer means of crossing at intersections.
Social - D. - Community Services - #1	None	The Doane Stuart School is located at the intersection of Washington Ave/ Forbes/ 7 th .
Economic – B. – Business Districts	None	The project will construct or modify pedestrian signals and re-construct curb ramps as needed depending upon existing conditions.

3.3 ANTICIPATED PERMITS/CERTIFICATIONS/COORDINATION

All the intersections are local roadways and will not require permitting from NYSDOT or the County.

Permits

- City of Rensselaer Road Opening Permit

Coordination

- Coordination with the City of Rensselaer
- Coordination with the New York State Department of Transportation
- Coordination with the Federal Highway Administration
- Coordination with the New York State Historic Preservation Officer (SHPO)

3.4 NYS SMART GROWTH PUBLIC INFRASTRUCTURE POLICY ACT (SGPIPA)

To the extent practicable this project has met the relevant criteria as described in ECL § 6-0107. The Smart Growth Screening Tool was used to assess the project's consistency and alignment with relevant Smart Growth criteria; the tool was completed by the Region's Planning and Program Management group on and reflects the current project scope.

4.1 FUNDING

FUNDING SOURCE: ☐ 100% State

☒ Federal

MPO INVOLVEMENT:

☐ No

☒ Yes

TIP Name: City of Rensselaer PSAP TIP No.: R327

TIP AMENDMENT REQUIRED: ☒ No ☐ Yes; Needed by:STIP STATUS: ☒ On STIP ☐ Not on STIP**4.2 COST AND SCHEDULE**

- ☐ Public Meeting ☐ 4(f)/106 FHWA sign-off
☐ Permits ☐ Consultant(s) for:
☐ Other – Identify e.g., utilities, endangered species (ESA)

Schedule and Cost				
Project Phase	Activity Duration	Estimated Cost	Fund Source	Obligation Date
Design V-VI	9/2019 - 11/2019	\$50,000	HSIP (100% Federal)	SFY 2020
Construction	4/2020 to 8/2020	\$395,000	HSIP (100% Federal)	SFY 2020
Construction Inspection	4/2020 to 8/2020	\$55,000	HSIP (100% Federal)	SFY 2020
TOTAL ESTIMATED COST		\$500,000		

BASIS OF ESTIMATE: TIP**PROGRAM DISPOSITION/LETTING:** Scheduled for letting in SFY 2020**STATEWIDE SIGNIFICANCE:** ☒ No Remarks:

Design approval is scheduled for July of 2020 with construction scheduled to begin in April of 2020 and last four months.

Project Schedule	
Activity	Date Occurred/Tentative
Scope Approval	January 2019
Design Approval	July 2019
ROW Acquisition	Not required
Construction Start	April 2020
Construction Complete	August 2020

Project Cost (in millions)		
Activities		Reasonable/Preferred Alternative (Alternative 1)
Construction Costs	Bridge	\$0
	Highway	\$308,700
	Field Change Item	\$18,200
Incidentals (10%)		\$36,300
Subtotal 1		\$363,200
Contingency (15% at Design Approval)		\$64,000
Mobilization (4%)		\$17,800
Subtotal 2		\$445,000
Expected Award Amount (Inflate current costs/prices at 0%/yr. to midpoint of construction to arrive at \$ amount to be entered here) See HDM 21.6.3.2 B		\$0
Construction Inspection (9%)		\$55,000
ROW Costs		\$0
Total Alternative Costs		\$500,000

5.1 PUBLIC INVOLVEMENT

Notifications to public officials, potential stakeholders and emergency responders and schools have been completed.

Public Involvement Plan Schedule of Milestone Dates	
Activity	Date Occurred/Tentative
Stake holder Meeting	Not Required
Focus Group Meeting	Not Required
Meeting with City Reps.	March 2019
Meeting with Agency (SHPO/USCG/USACE, etc.)	Not Required

Public Involvement Plan Schedule of Milestone Dates	
Activity	Date Occurred/Tentative
Public Informational Meeting	Not Required

6.1 LIST OF ATTACHMENTS / APPENDCIES

Project Appendices

Appendix A – Project Plans

Appendix B – Environmental Documents

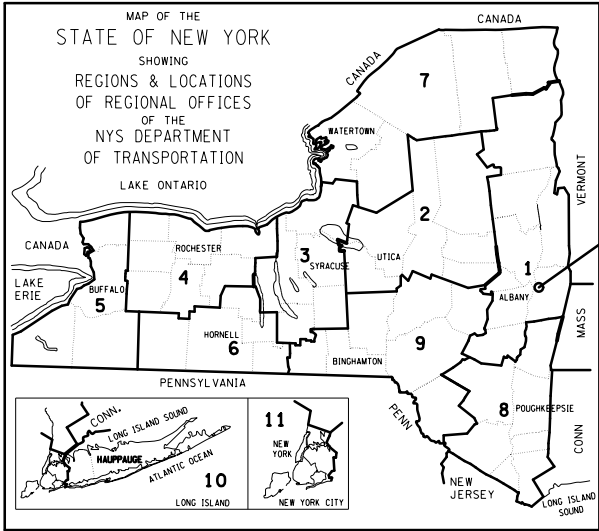
- Endangered Species Act Transmittal Sheet
- Section 106 Concurrence Memorandum (Coordination ongoing)
- Federal Environmental Approval Worksheet (FEAW)
- Social, Economic and Environmental Resources Checklist (SEERC)

Appendix C – Traffic Information

- Complete Streets Checklist
- NYSDOT Smart Growth Screening

Appendix A

Plans, Profiles, and Typical Sections



PROJECT LOCATION



RENSSELAER PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)

CITY OF RENSSELAER,
RENSSELAER COUNTY

JUNE, 2019

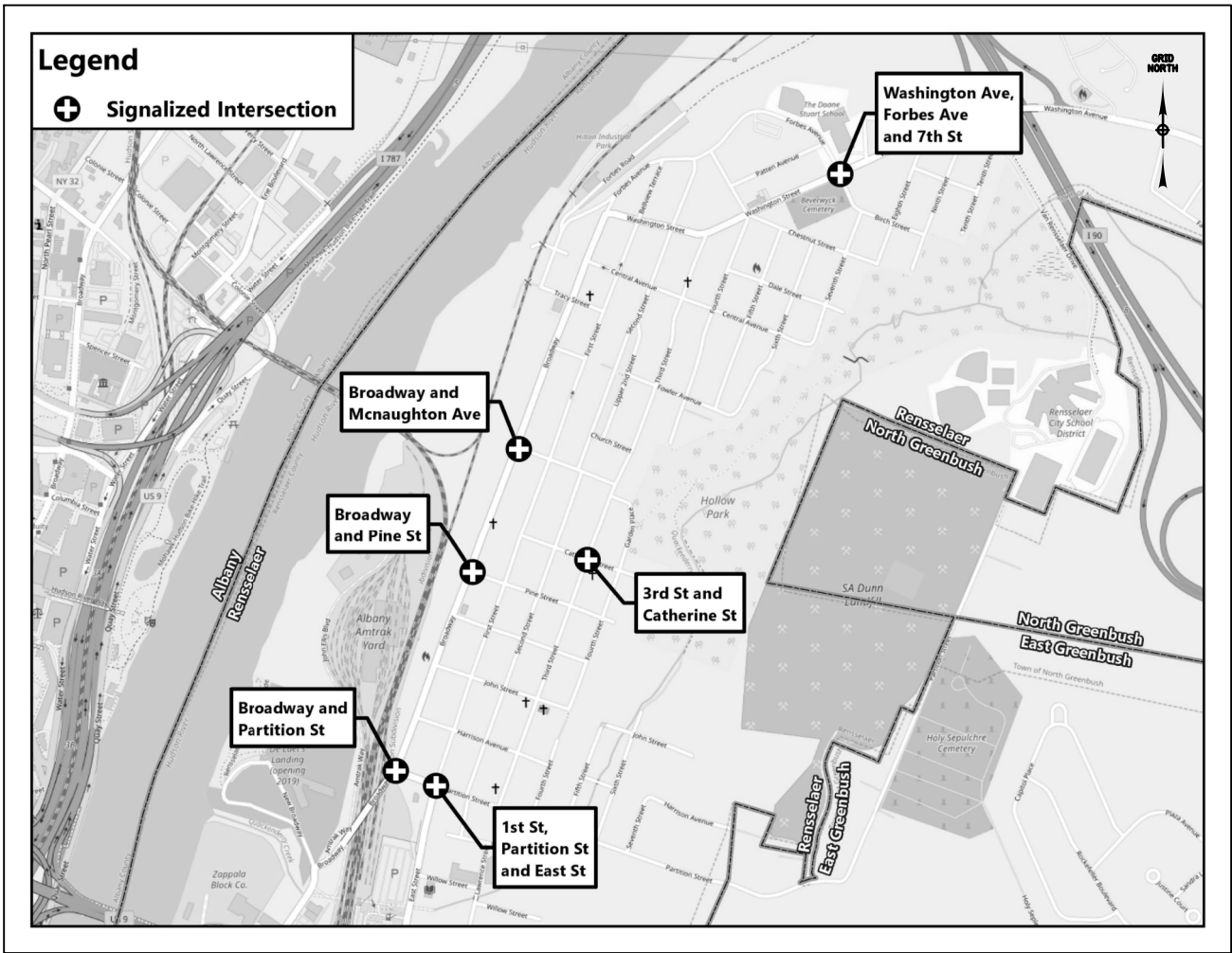
PRELIMINARY PLANS

XX SHEETS

PIN 1760.94/DXX

RENSSELAER COUNTY

INTERSECTIONS	
1	WASHINGTON AVENUE/FORBES AVENUE/7TH STREET
2	BROADWAY/PARTITION STREET
3	BROADWAY/PINE STREET
4	BROADWAY/MCNAUGHTON AVENUE
5	1ST STREET/PARTITION STREET/EAST STREET
6	3RD STREET/CATHERINE STREET



PROJECT LOCATION
NOT TO SCALE

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL."

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYSDOT POLICIES AND GUIDELINES AND THE FINAL DESIGN REPORT APPROVED ON XXX.

PREPARED AND
RECOMMENDED BY:

PETER FAITH, P.E.
VICE PRESIDENT/DIRECTOR OF TRAFFIC ENGINEERING

MICHAEL WIESZCHOWSKI, P.E., QA/QC
SENIOR TRANSPORTATION ENGINEER

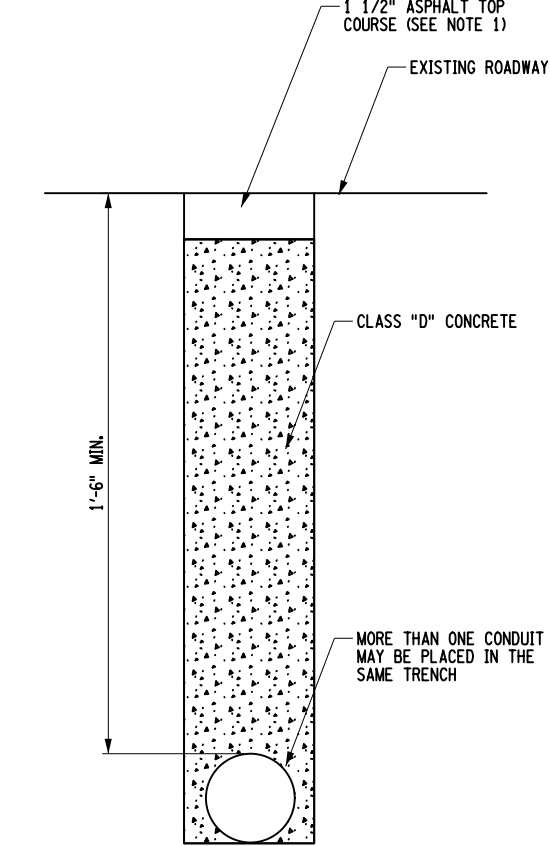
GPI
GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS
80 WOLF ROAD, SUITE 300
ALBANY, NY 12205

RENSSELAER PEDESTRIAN SAFETY IMPROVEMENTS
(VARIOUS LOCATIONS)
CITY OF RENSSELAER
COUNTY: RENSSELAER

FED. ROAD REG. NO.	STATE	SHEET NO.
1	N.Y.	1

CAPITAL PROJECT
IDENTIFICATION NO. 1760.94

INDEX ON SHEET NO. 2



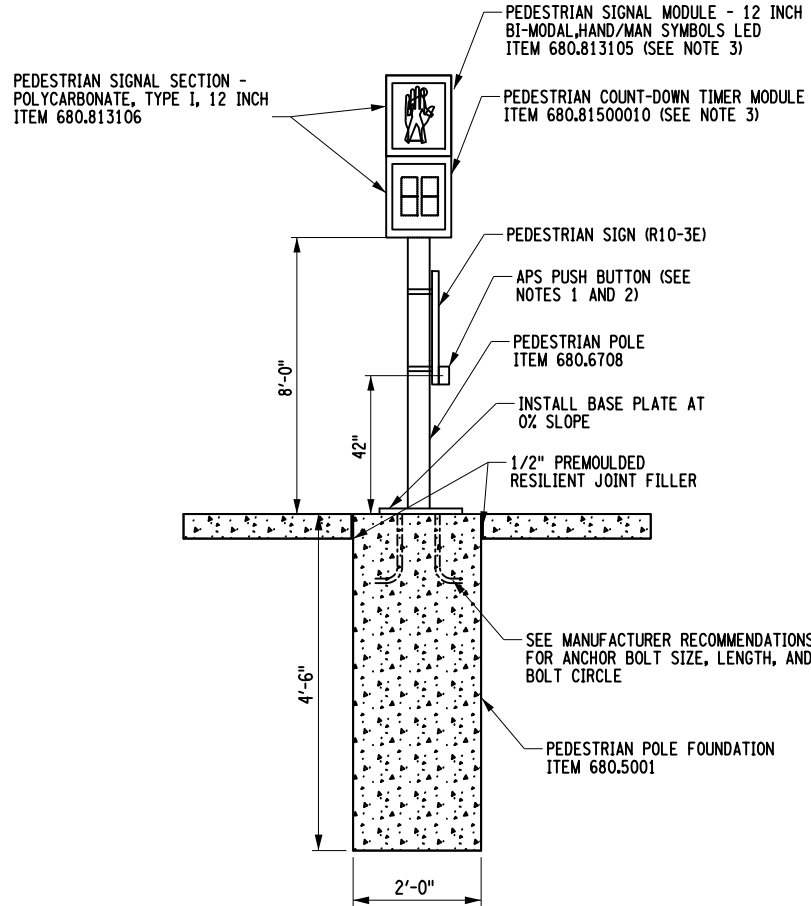
CONDUIT EXCAVATION AND BACKFILL IN PAVEMENT
(NTS)

NOTES:

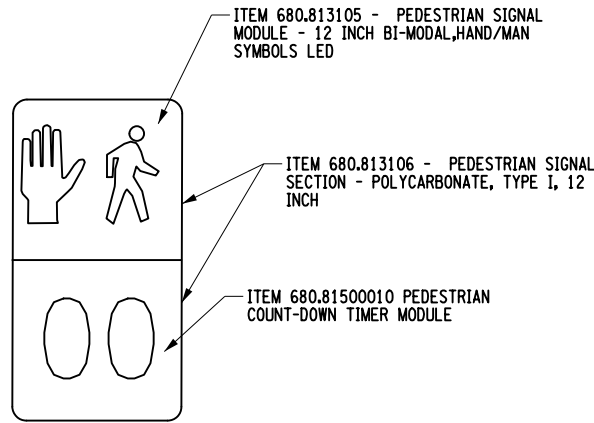
1. ALL EXCAVATION, BACKFILL, AND RESTORATION IS TO BE PAID FOR UNDER THE BID PRICE FOR 206.03 - CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION.

NOTE:

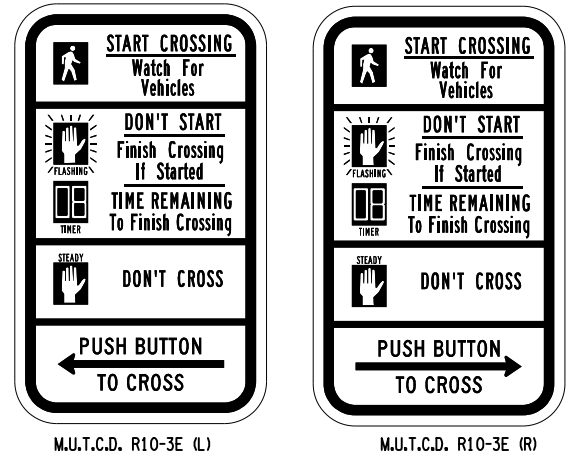
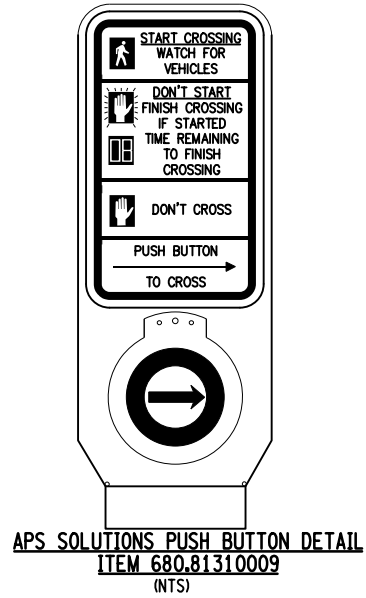
1. SEE THE TRAFFIC SIGNAL PLANS FOR PUSH BUTTON ORIENTATION. THE PUSH BUTTON IS TO FACE AN ACCESSIBLE SURFACE OR BE WITHIN 10' OF AN ACCESSIBLE SURFACE. AN ACCESSIBLE SURFACE IS DEFINED AS A CONCRETE/ASPHALT SURFACE.
2. IF TWO APS PUSHBUTTONS WITH SIGNS ARE TO BE INSTALLED ON THE SAME POLE WITH A 90 DEGREE ORIENTATION, THE PUSHBUTTONS SHOULD BE PLACED AS CLOSE TO 90 DEGREES AS POSSIBLE, BUT NO MORE THAN 110 DEGREES APART.
3. A STAINLESS STEEL TOP PLATE SHALL BE ATTACHED TO THE TOP OF EACH BRACKET MOUNTED ASSEMBLY.



PEDESTRIAN SIGNAL POLE DETAIL
(NTS)




PEDESTRIAN SIGNAL DETAIL
(NTS)

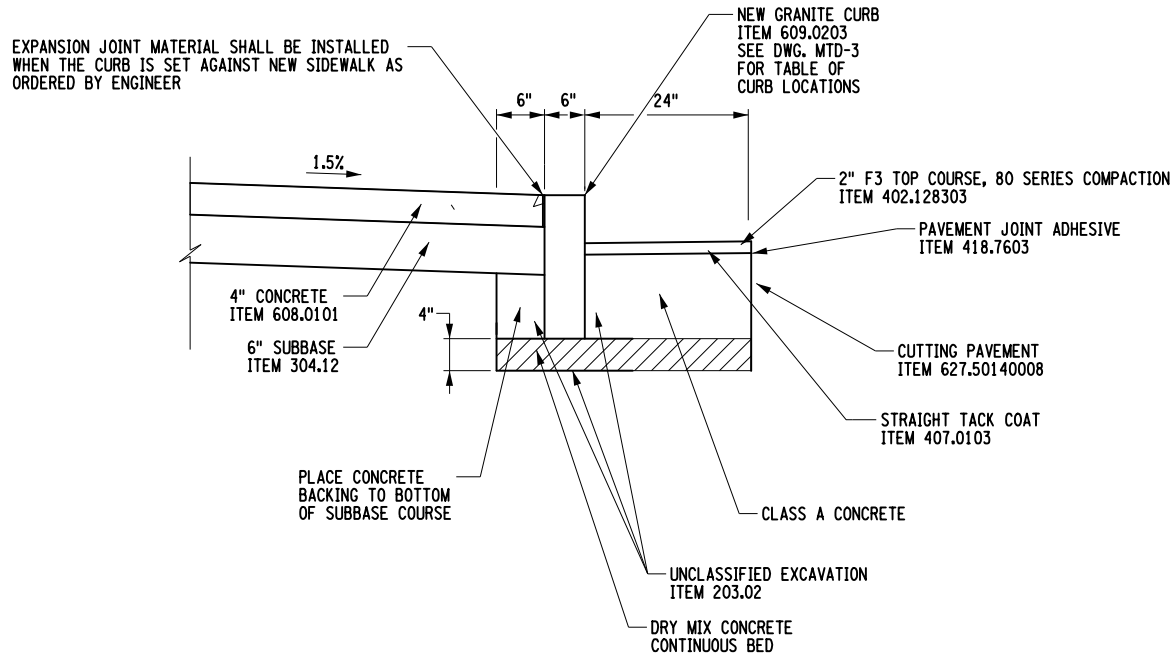
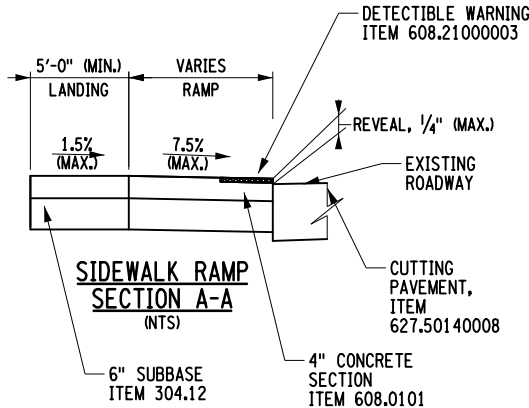
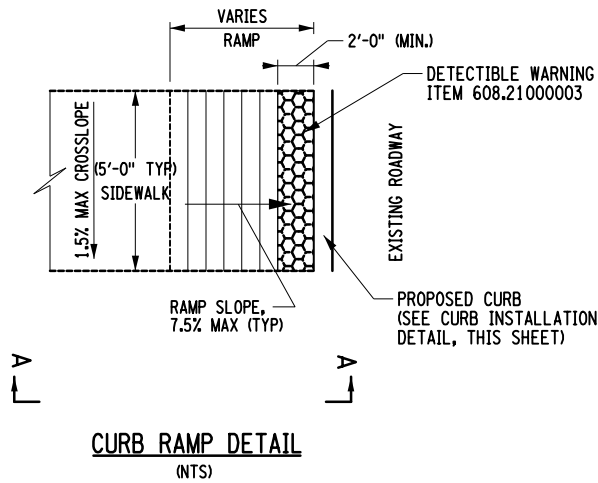


NOTE:

1. THE PRICE FOR THE PEDESTRIAN SIGNAL SIGN SHALL BE INCLUDED IN THE BID PRICE FOR 680.81310009.

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:	ALTERED BY: ON:

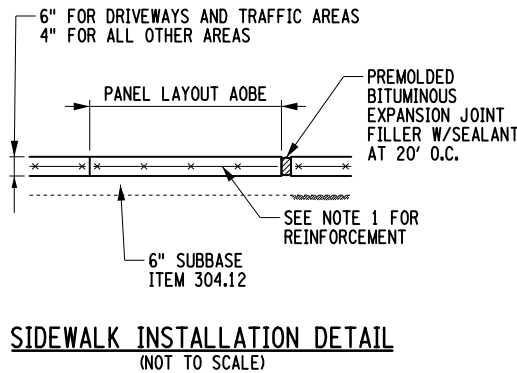
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)	PROJECT: 2019-04 UTIL QLVL = D	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CITY OF RENSSELAER				MISCELLANEOUS DETAILS	DRAWING NO. TSD-1
						SHEET NO. 8
	COUNTY: RENSSELAER					
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					<div><div>GPI</div><div>GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS</div></div>	<div><div>CITY OF RENSSELAER</div></div>



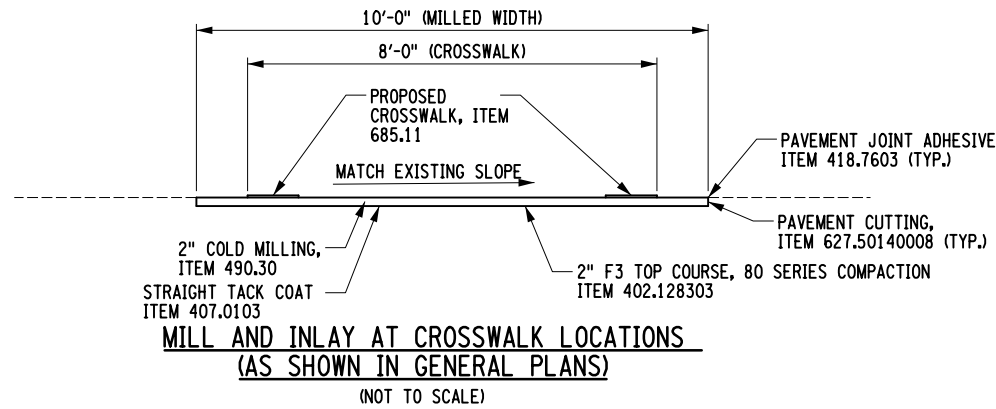
DESIGN ELEMENT TOLERANCES		
ELEMENT	DESIGN AND FIELD LAYOUT LIMIT	LIMIT FOR WORK ACCEPTANCE
SIDEWALK CROSS SLOPE	1.5% MAX.	2.0% MAX.
SIDEWALK GRADE (RUNNING SLOPE)	4.5% MAX.	5.0% MAX.
CURB RAMP GRADE (RUNNING SLOPE)	7.5% MAX.	8.3% MAX.
BLENDED TRANSITION GRADE (RUNNING SLOPE)	4.5% MAX.	5.0% MAX.

NOTE:
1. REFER TO NYSDOT STANDARD SHEETS 608-01.

CURB AND SIDEWALK DESIGN TOLERANCES
(NTS)



NOTES:
1. THE CONTRACTOR IS TO USE CLASS D CONCRETE WITH REINFORCEMENT PER 608-2.01D OF THE NYS STANDARD SPECIFICATIONS. CONTRACTORS ARE GIVEN THE OPTION OF USING 6"X6" WIRE MESH (W2.9 OR 23) OR USING FIBER REINFORCEMENT. WHERE THERE IS A COMMERCIAL DRIVEWAY, THE CONTRACTOR SHALL USE WIRE MESH WITH THE OPTION OF USING ADDITIONAL FIBER REINFORCEMENT.



AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)	PROJECT: 2019-04 UTIL QLVL = D	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER		
	CITY OF RENSSELAER					DRAWING NO. TSD-2 SHEET NO. 9		
					MISCELLANEOUS DETAILS			
	COUNTY: RENSSELAER							
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					GPI GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS		CITY OF RENSSELAER	

FILE NAME = \$FILEABBREV\$
DATE/TIME = \$DATE\$
USER = \$NTUSER\$

DESIGN SUPERVISOR F. MASTROIANNI

JOB MANAGER M. WIESZCHOWSKI

DESIGN J. GENTZLER

CHECK

M. WIESZCHOWSKI

DRAFTING J. GENTZLER

CHECK D. GREEN

PROJECT MANAGER M. WIESZCHOWSKI

NOTES:

1. CONDUIT TRENCH ACROSS ROADWAY SHALL BE MADE WITHIN THE MILL AND FILL CROSSWALK AREA TO MINIMIZE ROADWAY DAMAGE.
2. CONTROLLER CABINET SHALL BE MODIFIED TO ACCEPT NEW PEDESTRIA SIGNAL EQUIPMENT WIRING AND SIGNAL CONTROLLERS SHALL BE REPROGRAMMED FOR NEW PEDESTRIAN PHASES. ALL LABOR AND MATERIALS NECESSARY FOR THIS WORK SHALL BE PAID FOR UNDER ITEM 680.77000105.
3. UNDER ITEM 680.81310109, THE POST PROVIDED IS TO BE A 4" DIAMETER ALUMINUM POLE THAT IS IDENTICAL TO THOSE PROVIDED UNDER ITEM 680.6708. THE POST IS TO BE POWDER COATED BLACK WITH A CAP AND FLUSH MOUNTED TO THE SIDEWALK (NO TRANSFORMER BASE).

AFFIX SEAL: GREENMAN - PEDERSEN, INC
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)

CITY OF RENSSELAER

COUNTY: RENSSELAER

PROJECT: 2019-04

UTIL QLVL = D

BRIDGES

CULVERTS

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER

GENERAL PLAN
WASHINGTON AVE/FORBES AVE/7TH ST

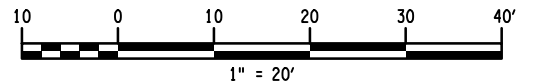
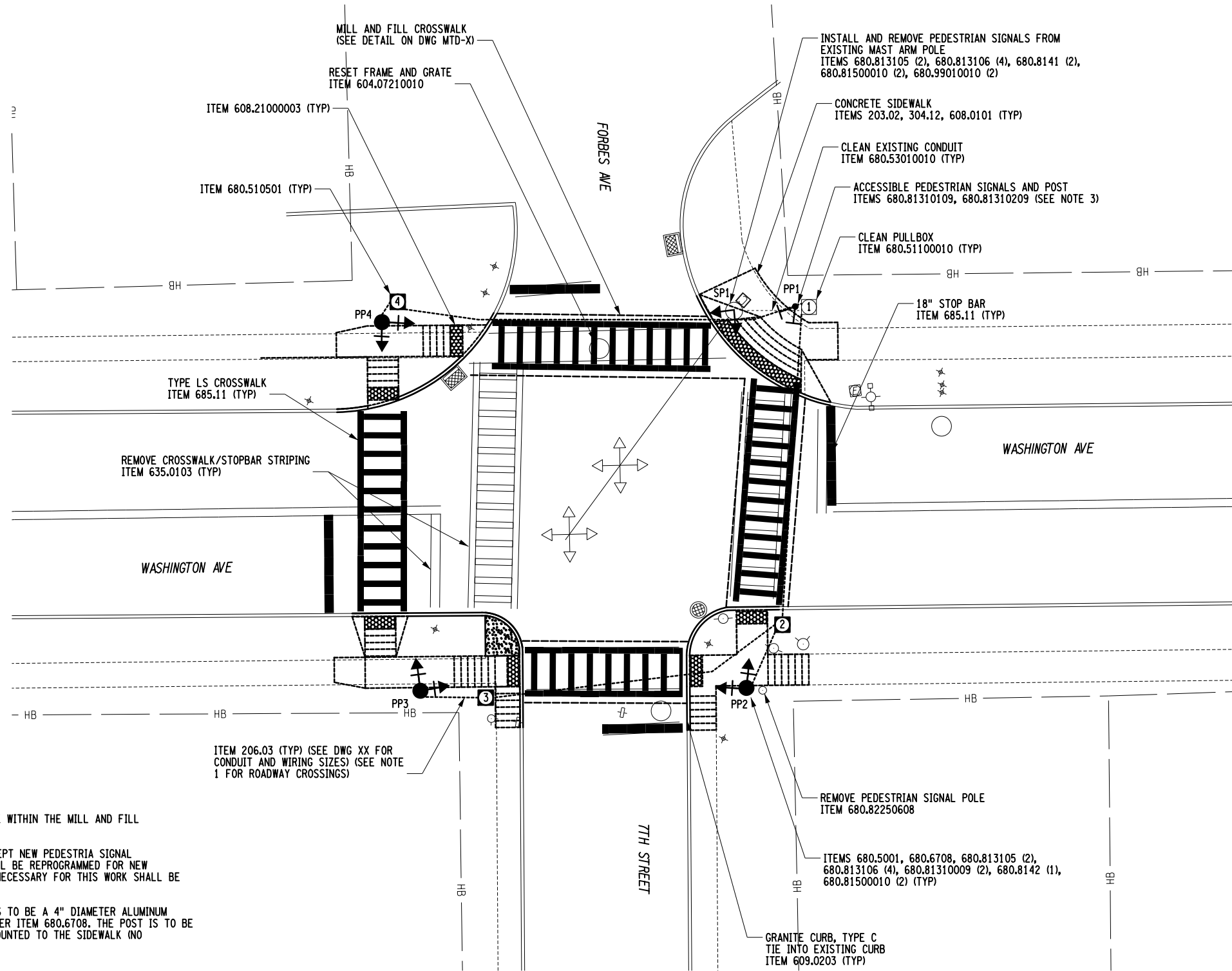
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SHEET NO. 11

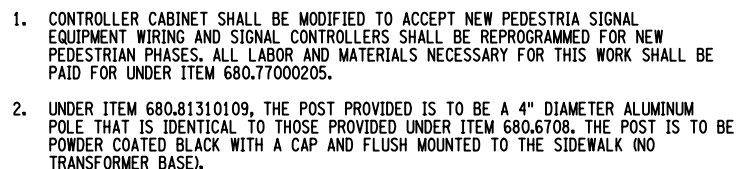
GPI GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS




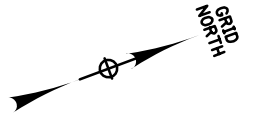
CITY OF RENSSELAER

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.





AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:		ALTERED BY: ON:		<div><div><div>10</div><div>0</div><div>10</div><div>20</div><div>30</div><div>40'</div></div><div>1" = 20'</div></div>											
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)		PROJECT: 2019-04		BRIDGES		CULVERTS		ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED		CONTRACT NUMBER	
				CITY OF RENSSELAER		UTIL QLVL = D									
				COUNTY: RENSSELAER											
		IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.										<div><div><div>GPI</div><div>GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS</div></div><div></div></div>		CITY OF RENSSELAER	



10 0 10 20 30 40'

1" = 20'

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:		ALTERED BY: ON:		<div><div><div>10</div><div>0</div><div>10</div><div>20</div><div>30</div><div>40'</div></div><div>1" = 20'</div></div>													
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)		PROJECT: 2019-04		BRIDGES		CULVERTS		ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED		CONTRACT NUMBER			
				CITY OF RENSSELAER		UTIL QLVL = D						GENERAL PLAN BROADWAY/PINE ST		DRAWING NO. GNP-3 SHEET NO. 13			
		COUNTY: RENSSELAER															
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.														<div><div><div>GPI</div><div>GREENMAN-PEDERSEN, INC.</div><div>CONSULTING ENGINEERS</div></div></div>		<div><div><div>CITY OF RENSSELAER</div><div></div></div></div> <div>CITY OF RENSSELAER</div>	

FILE NAME = \$FILEABBREV\$
DATE/TIME = \$DATE\$
USER = \$NTUSER\$

DESIGN SUPERVISOR F. MASTROIANNI

JOB MANAGER M. WIESZCHOWSKI

DESIGN J. GENTZLER

CHECK

M. WIESZCHOWSKI

DRAFTING J. GENTZLER

CHECK D. GREEN

PROJECT MANAGER M. WIESZCHOWSKI

NOTES:

- CONDUIT TRENCH ACROSS ROADWAY SHALL BE MADE WITHIN THE MILL AND FILL CROSSWALK AREA TO MINIMIZE ROADWAY DAMAGE.
- CONTROLLER CABINET SHALL BE MODIFIED TO ACCEPT NEW PEDESTRIA SIGNAL EQUIPMENT WIRING AND SIGNAL CONTROLLERS SHALL BE REPROGRAMMED FOR NEW PEDESTRIAN PHASES. ALL LABOR AND MATERIALS NECESSARY FOR THIS WORK SHALL BE PAID FOR UNDER ITEM 680.77000405.
- UNDER ITEM 680.81310109, THE POST PROVIDED IS TO BE A 4" DIAMETER ALUMINUM POLE THAT IS IDENTICAL TO THOSE PROVIDED UNDER ITEM 680.6708. THE POST IS TO BE POWDER COATED BLACK WITH A CAP AND FLUSH MOUNTED TO THE SIDEWALK (NO TRANSFORMER BASE).

AFFIX SEAL: GREENMAN - PEDERSEN, INC
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)

CITY OF RENSSELAER

COUNTY: RENSSELAER

PROJECT: 2019-04

UTIL QLVL = D

BRIDGES

CULVERTS

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER

GENERAL PLAN
BROADWAY/McNAUGHTON AVE

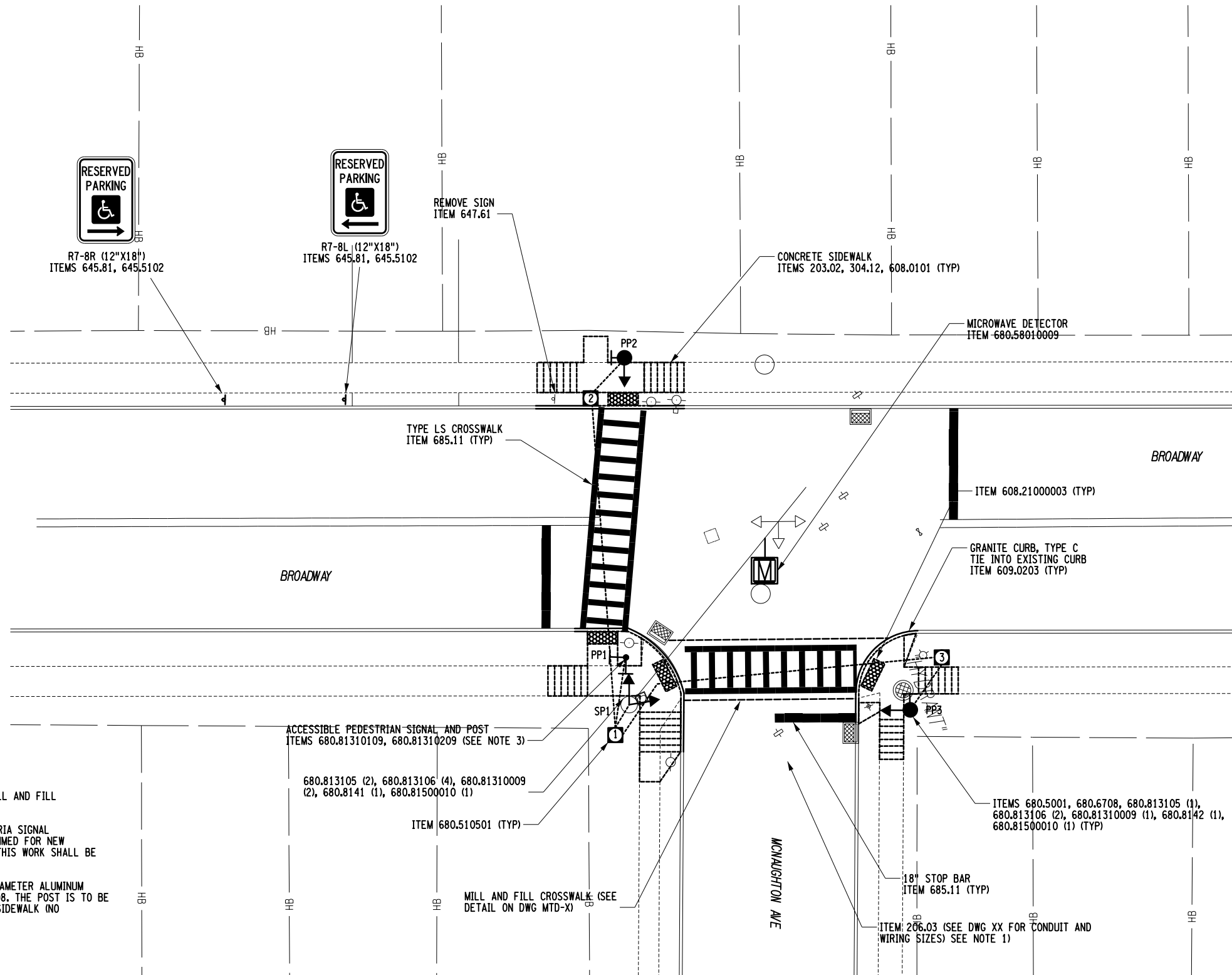
DRAWING NO. GNP-4
SHEET NO. 14

GPI

GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS



CITY OF RENSSELAER



1" = 20'

FILE NAME = \$FILEABBREV\$
DATE/TIME = \$DATE\$
USER = \$NTUSER\$

DESIGN SUPERVISOR F. MASTROIANNI

JOB MANAGER M. WIESZCHOWSKI

DESIGN J. GENTZLER

CHECK

M. WIESZCHOWSKI

DRAFTING J. GENTZLER

CHECK D. GREEN

PROJECT MANAGER M. WIESZCHOWSKI

NOTES:

- THE BID PRICE FOR ITEM 680.77000505 IS TO INCLUDE THE FOLLOWING WORK:
 - CONTROLLER CABINET SHALL BE MODIFIED TO ACCEPT NEW PEDESTRIA SIGNAL EQUIPMENT WIRING AND SIGNAL CONTROLLERS SHALL BE REPROGRAMMED FOR NEW PEDESTRIAN PHASES. ALL LABOR AND MATERIALS NECESSARY FOR THIS WORK SHALL BE PAID FOR UNDER ITEM 680.77000505.
 - IT IS ANTICIPATED THAT AS A RESULT OF THE ASPHALT REMOVAL AROUND THE BASE OF SPAN WIRE POLE SP1, A GAP WILL BE LEFT BETWEEN THE BASE AND PROPOSED CONCRETE CURB RAMP. THE CONTRACTOR IS TO FILL THIS GAP WITH MORTAR TO CREATE A VERTICAL FACE THAT IS FLUSH TO THE BASE.
- UNDER ITEM 680.81310109, THE POST PROVIDED IS TO BE A 4" DIAMETER ALUMINUM POLE THAT IS IDENTICAL TO THOSE PROVIDED UNDER ITEM 680.6708. THE POST IS TO BE POWDER COATED BLACK WITH A CAP AND FLUSH MOUNTED TO THE SIDEWALK (NO TRANSFORMER BASE).

AFFIX SEAL: GREENMAN - PEDERSEN, INC
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)

CITY OF RENSSELAER

COUNTY: RENSSELAER

PROJECT: 2019-04

UTIL QLVL = D

BRIDGES

CULVERTS

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER

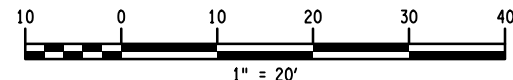
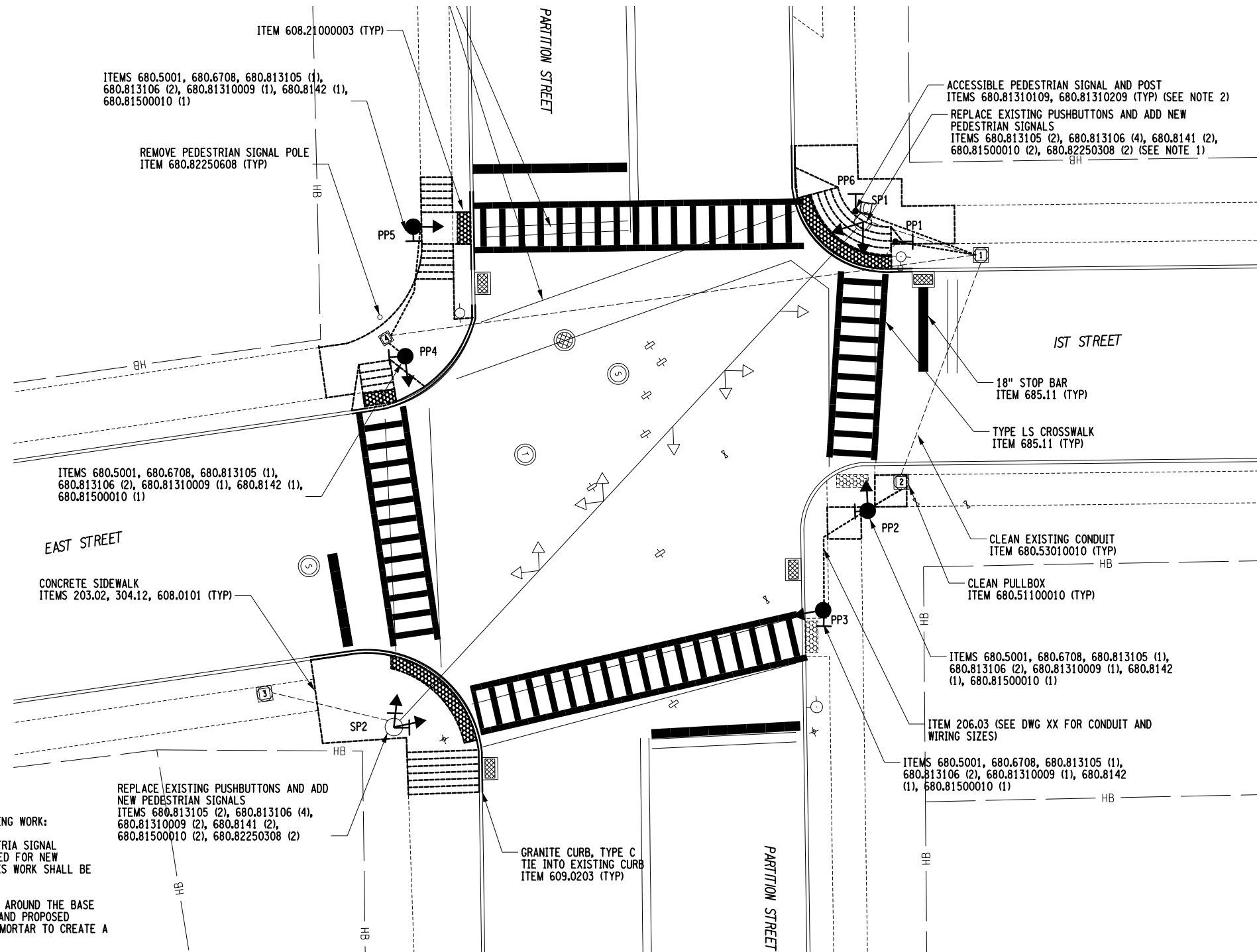
GENERAL PLAN
1ST ST/PARTITION ST/EAST ST

DRAWING NO. GNP-5
SHEET NO. 15

GPI GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS



CITY OF RENSSELAER

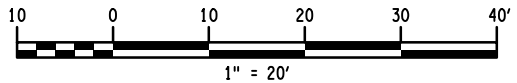
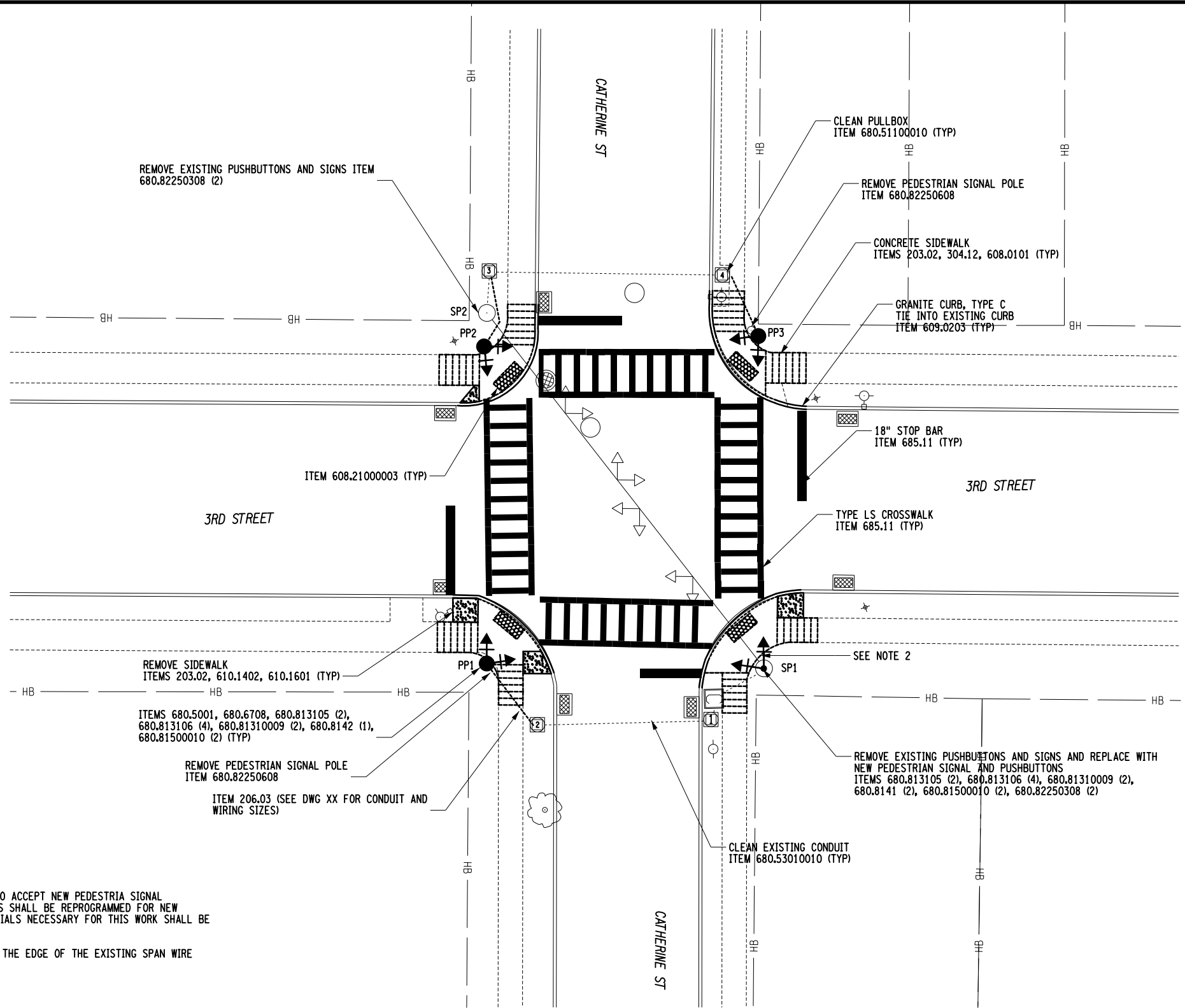


FILE NAME = \$FILEABBREV\$
DATE/TIME = \$DATE\$
USER = \$NTUSER\$

DESIGN SUPERVISOR F. MASTROIANNI
JOB MANAGER M. WIESZCHOWSKI
DESIGN J. GENTZLER
CHECK M. WIESZCHOWSKI
DRAFTING J. GENTZLER
CHECK D. GREEN
PROJECT MANAGER M. WIESZCHOWSKI

NOTES:

1. CONTROLLER CABINET SHALL BE MODIFIED TO ACCEPT NEW PEDESTRIA SIGNAL EQUIPMENT WIRING AND SIGNAL CONTROLLERS SHALL BE REPROGRAMMED FOR NEW PEDESTRIAN PHASES. ALL LABOR AND MATERIALS NECESSARY FOR THIS WORK SHALL BE PAID FOR UNDER ITEM 680.77000605.
2. THE CONTRACTOR IS TO FORM SIDEWALK TO THE EDGE OF THE EXISTING SPAN WIRE POLE.



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

PEDESTRIAN SAFETY IMPROVEMENTS (VARIOUS LOCATIONS)

CITY OF RENSSELAER

COUNTY: RENSSELAER

PROJECT: 2019-04

UTIL QLVL = D

BRIDGES

CULVERTS

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER

GENERAL PLAN
3RD ST/CATHERINE ST

DRAWING NO. GNP-6
SHEET NO. 16

GPI GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS



CITY OF RENSSELAER

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Appendix B

Environmental Information

Endangered Species Act

1. Federal Highway Administration Endangered Species Act Transmittal Sheet

State Historic Preservation Office (SHPO)

2. SHPO Concurrence - TBD

Other Forms

3. Social, Economic, and Environmental Checklist
4. Federal Environmental Approvals Worksheet

**Section 7 ESA Process:
ESA Transmittal Sheet**

Section 7 ESA Process: ESA/EFH Transmittal Sheet

Step 3: Documentation. Please complete the appropriate boxes below and complete the documentation as described.

	ESA/EFH Does Not Apply	No Effect, Activity-Based	No Effect, No Suitable Habitat or No Effect	BATS: MA, NLAA, 14-Day Form, or IPaC Submittal	NLEB: MA, LAA 30 Day Form, or IPaC Submittal	MA, NLAA, Traditional 7-step Process	MA, LAA, Formal Consultation
Northern Long-eared Bat		15 / 24					
Indiana Bat					NA		
Bog Turtle				NA	NA		
Mollusks (Dwarf Wedge Mussel, Rayed Bean, Clubshell, Chittenango Ovate Amber Snail)				NA	NA		
Karner Blue Butterfly				NA	NA		
Sturgeon (Shortnose, Atlantic)				NA	NA		
Sea Turtles				NA	NA		
Atlantic Large Whales			NA	NA	NA		
EFH Resources (circle one)	EFH Does Not Apply	No Effect, Activity-Based	NA	NA	EFH Programmatic Agreement Applies	EFH Programmatic Agreement MAY Apply	Individual EFH Consultation is Required
Documentation Required	The IPaC/NMFS ESA/EFH Mapper report is included in the Design Report.	Record the corresponding number of the activity in the box above. This sheet and the IPaC/NMFS ESA/EFH printout are included in the Design Report.	NYSDOT submits "No Effect, No Suitable Habitat Determination" to FHWA. Concurrence has been obtained if 15 days passes without correspondence from FHWA.	NYSDOT submits 14-day Form to USFWS-cc: Area Engineer, OR submits through IPaC w/Area Engineer included.	NYSDOT submits 30-day Form to FHWA-then to USFWS, OR NYSDOT submits through IPaC w/ Area Engineer included.	NYSDOT submits either BE or BA to FHWA, who submits to USFWS for concurrence.	NYSDOT submits BA to FHWA for Initiation of Formal Consultation with USFWS or NMFS.

Instructions for Use: This Summary Sheet is sent to FHWA for concurrence for all submissions, except "ESA Does Not Apply" and "No Effect, Activity-Based". A submittal package includes all documentation for all species requiring concurrence, with a cover letter requesting concurrence, so that FHWA can make one ESA determination. SEE EACH SPECIES-SPECIFIC PACKAGE FOR SPECIFIC DOCUMENTATION REQUIREMENTS FOR SUBMITTALS. Also, FHWA requires documentation of compliance with ESA in the Design Report.

Highway/Bridge Work Types

1. Joint and Crack Sealing
2. Pavement Marking
3. Impact Attenuator Repair or Replacement
4. Repair and Replace Loop Detectors
5. On-call Guide Rail Repair
6. Rigid Pavement Repairs (spall repair, grinding, etc.)
7. Pavement Grooving
8. Microsurfacing and Chip Sealing
9. Shoulder Rumble Strip Installation
10. Delineator and/or Reference Marker Placement or Replacement
11. Graffiti Removal and/or Prevention
12. Shoulder Rehabilitation and/or Repair
13. Traffic Management Systems Maintenance (communications cable, hardware for ITS, RWIS, etc.)
14. Highway Lighting Upgrading (excluding luminaire replacement and installation of high mast lighting)
15. Bicycle Path and Walkway Rehabilitation (e.g. ADA curb ramps)
16. Install, Replace and/or Repair Permanent Traffic Count Detectors
17. Install, Replace and/or Repair Weigh-in-Motion Detectors
18. Recharge Basin Reconditioning
19. Underdrain Installation
20. Guide Rail and/or Median Barrier Upgrading (including placement of new guide railing or median barrier)
21. Upgrading Sign(s) and/or Traffic Signal(s)
22. Install, Replace and/or Repair Right-Of-Way, Pedestrian and Permanent Snow Fencing
23. Park and Ride Lot Rehabilitation
24. 1R Projects that do not involve drainage work or work off of the paved surface/shoulder
25. 2R Projects that do not involve drainage work or work off of the paved surface/shoulder
26. 3R Projects (freeway and non-freeway) that do not involve drainage work, bridge work, or work off of the paved surface/shoulder
27. Highway-Rail Grade Crossing: installation of new and/or replacement of existing automatic warning devices
28. Highway-Rail Grade Crossing: installation of new and/or replacement of existing signage
29. Highway-Rail Grade Crossing: interconnection of grade crossing warning systems with vehicular traffic signal system
30. Highway-Rail Grade Crossing: surface replacement or upgrade
31. Bridge Work: Maintenance, alteration, and demolition of bridges/structures from October 1 to March 31 that does not include alteration of permanent street lighting and does not alter bat roost potential, or involve any work within rivers, streams, or wetlands, OR the same type of work between April 1 and September 30 that has negative Bridge/Bat Survey results.
32. Maintenance and Protection of Traffic (M&PT) activities within the operational right-of-way.

Social, Economic and Environmental Resources Checklist

PIN:1760.94	FUNDING TYPE: HSIP
DESCRIPTION: PEDESTRIAN SAFETY IMPROVEMENTS AT VARIOUS LOCATIONS (CITY OF RENSSELAER)	DATE:5/21/19
	REVISION DATE:
MUNICIPALITY: CITY OF RENSSELAER	NEPA CLASS: Class II Cat Ex
COUNTY: RENSSELAER	SEQRA TYPE: Type II
SCOPE: The project will either replace or install pedestrian signals and curb ramps at (6) signalized intersections. Depending upon project funding one or two traffic signals will be replaced to meet current standards. There will also be miscellaneous improvements to striping, sidewalks, and signage.	

SOCIAL, ECONOMIC AND ENVIRONMENTAL CONSIDERATIONS	IF YES, GO TO IMPACT OR ISSUE; IF NO CHECK BOX BELOW	IMPACT ¹ OR ISSUE?	
	NO	YES	NO
Social			
A. Land Use			
1. Is there potential to affect current land use/zoning?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Is there a lack of consistency with community's comprehensive plan and/or other local or regional planning goals?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Will the project affect any planned or future development?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
B. Neighborhoods and Community Cohesion			
1. Are relocations of homes or businesses proposed or acquisition of community resources anticipated?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Is there potential for changes to neighborhood character?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Is there a potential to impact transportation options (e.g., transit, walking, bicycling)? <i>Focuses on improving pedestrian safety</i>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Are there potential changes to travel patterns that could affect neighborhood quality of life?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Will the project divide or isolate portions of the community or generate new development that could affect the current community structure?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C. General Social Groups			
1. Are there potential effects to the ability of transit dependent, elderly, or disabled populations to access destinations (particularly local businesses and health care facilities)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Does the project have the potential to disproportionately impact low income or minority populations (Environmental Justice)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Are there alterations to pedestrian facilities that would affect the elderly or disabled such as lengthening pedestrian crossings or providing median refuge?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
D. Community Services			
1. Is there potential to affect access to or use of Schools, Recreation Areas or Places of Worship (e.g., detours, sidewalk removal, addition of curb ramps, crosswalks, pedestrian signals,	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

SOCIAL, ECONOMIC AND ENVIRONMENTAL CONSIDERATIONS	IF YES, GO TO IMPACT OR ISSUE; IF NO CHECK BOX BELOW	IMPACT ¹ OR ISSUE?	
	NO	YES	NO
etc.)? Project is near Shenendehowa High School.			
2. Is there potential to affect emergency service response?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Economic			
A. Regional and Local Economies			
1. Is there potential to affect local economic viability (e.g., development potential, tax revenues, employment opportunities, retail sales or public expenditures)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Is there a potential to divert traffic away from businesses?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
B. Business Districts			
1. Are there potential effects on the viability or character of Business Districts?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Will the project affect transportation options available for patrons getting into or out of the District?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Will sidewalks, bicycling opportunities or transit opportunities to or within the district be affected? <i>Focuses on improving pedestrian safety</i>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Will parking within the district be affected?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
C. Specific Business Impacts			
1. Are effects to specific businesses anticipated? (e.g., sidewalks, bicycling opportunities, or handicapped access to and from businesses)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Will the project affect available transportation options for patrons to businesses?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Will the project affect the ability of businesses to receive deliveries?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Will parking for businesses be affected?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Environmental			
1. Are there wetlands within or immediately adjacent to the project limits? <i>See Environmental Procedures Manual (EPM) 4.A.R, Executive Order (EO) 11990 may apply.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Are there Surface Waters (other than wetlands) within or immediately adjacent to the project limits? <i>lakes, ponds streams or wetlands of any jurisdiction</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Is there a designated Wild or Scenic River within or immediately adjacent to the project limits? (See The Environmental Manual (TEM) 4.4.3)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Will the project require a U.S. Coast Guard Bridge Permit? <i>Project area includes a bridge over navigable waters of U.S.</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Does the project area contain waters regulated as Navigable by U. S. Army Corps of Engineers? <i>Section 404/10 Individual Permit or NWP 23 may be required</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. Is the project in a mapped Flood Zone? <i>TEM section 4.?, EO 11988</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. Is the project in or could it affect a designated coastal area? <i>FAN and/or Consistency determination may be required. See TEM 4.6</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

SOCIAL, ECONOMIC AND ENVIRONMENTAL CONSIDERATIONS	IF YES, GO TO IMPACT OR ISSUE; IF NO CHECK BOX BELOW	IMPACT ¹ OR ISSUE?	
	NO	YES	NO
8. Is the project area above a Sole Source Aquifer? See TEM 4.4 <i>Coordination with FHWA and/or EPA may be required.</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
9. Will the project involve one (1) acre of ground disturbance (or 5,000 sf in the East of Hudson watershed)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
10. Are federally/state listed endangered species or designated critical habitat indicated for the project county? <i>Coordination with DEC and/or a FHWA determination may be required. See TEM 4.4.9.3</i> <i>Threatened- Northern Long-eared Bat, Endangered- Karner Blue Butterfly</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. Is the project in a designated Critical Environmental Area? <i>TEM 4.4.11(SEQR issue)</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
12. Are there any resources protected by Section 106 (or Section 1409) within the project limits or immediate area? <i>See TEM 4.4.12 Appendix G</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
13. Is Native American coordination required outside of Section 106 consultation? <i>The project on or affecting Native American Lands or other areas of interest</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
14. Is there a use, constructive use or temporary occupancy of a 4(f) resource? <i>See SECTION 4(f) POLICY PAPER and contact Area Engineer.</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
15. Will the project involve conversion of a 6(f) resource? <i>listed as having Land and Water Conservation funds spent on the resource</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
16. Is there any potential to affect the character of important and possibly significant the visual resources of the project area and its environs? <i>(See PDM Chapter 3.2.2.2)</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
17. Will the project convert land protected by the Federal Farmland Protection Act? <i>See TEM 4.4.15</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
18. Will the project acquire active farmland from an Agricultural District? <i>(SEQR issue)</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
19. Is the project in a non-attainment area and exceed the CO screening criteria? <i>see EPM Chapter 1 1.1-19 an Air Quality Analysis required</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
20. Is the project in a non-attainment area and exceed the PM screening criteria? <i>see EPM Chapter 1 1.1-19? A hot spot analysis is required</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
21. Is the project a Type I Noise project as per 23 CFR 772? <i>See TEM 4.4.18</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
22. Will the project require the removal of Asbestos Containing Materials? <i>See TEM 4.4.19</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
23. Does the project area contain Contaminated and Hazardous Materials?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
24. Will the project increase the height of towers, construct new towers or other obstructions in a known migratory bird flyway?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

NOTES:

¹ The term "impacts" means both positive and negative effects. Both types of effects should be discussed in the body of the report as appropriate.

PREPARED BY (Print Name and Title): **Jeff Gentzler, P.E. - Project Engineer – Greenman-Pedersen, Inc.**

CERTIFICATION:

I certify that the information provided above is true and accurate.

Responsible Local Official _____ Date _____

Print Name and Title: _____

Federal Environmental Approval Worksheet

PIN: 1760.94	Completed by: JSG	Date Completed: 5/21/19	FUNDING TYPE: Federal
DESCRIPTION: PEDESTRIAN SAFETY IMPROVEMENTS AT VARIOUS LOCATIONS (CITY OF RENSSELAER)			NEPA CLASS: Class II: CE
			SEQR TYPE: Type II
LOCALITY (Village, Town, City): CITY OF RENSSELAER			COUNTY: RENSSELAER

Purpose of this Worksheet:

- Implement the Programmatic Agreement Between the Federal Highway Administration, New York Division (FHWA), and the New York State Department of Transportation (NYSDOT) Regarding the Processing of Actions Classified as Categorical Exclusions (CEs) for Federal-Aid Highway Projects (PARCE), executed September 2017.
- Communicate the project National Environmental Policy Act (NEPA) classification and identify whether the FHWA or the NYSDOT (titles identified per [Project Development Manual \(PDM\) Chapter 4, Exhibit 4-2](#)) is making the CE determination.
- Identify any FHWA independent determinations, approvals and/or concurrences required before the CE determination can be made.
- To be included within the Design Approval Document (DAD) in accordance with the documentation requirements in the PARCE.

Categorical Exclusion (CE) - a category of actions which do not individually or cumulatively have a significant effect on the human environment and which have been found to have no such effect in procedures adopted by a Federal agency (40 CFR 1508.4). Actions that do not individually or cumulatively have a significant environmental effect are excluded from the requirement to prepare an Environmental Assessment (EA) or Environmental Impact Statement (EIS) (23 CFR 771.115(b)).

Instructions:

Initial review of the Federal Environmental Approval Worksheet (FEAW) should occur in scoping or early in Design Phase I to identify potential risks. Complete new review of the FEAW periodically, particularly if project parameters or site condition changes result in potential resource impacts. Completion of the FEAW with signature in Step 4 is required prior to Design Approval. See PDM Chapter 4 for additional details.

Step 1A: Unusual Circumstances Threshold Determination – 23 CFR 771.117(b)

Do any, or the potential for any, unusual circumstances exist?

- | | |
|---|---|
| • Significant environmental impacts | YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> |
| • Substantial controversy on environmental grounds | YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> |
| • Significant impact on properties protected by Section 4(f) of the DOT Act or Section 106 of the National Historic Preservation Act | YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> |
| • Inconsistencies with any Federal, State, or local law, requirement or administrative determination relating to the environmental aspects of the project | YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> |

If yes to any of the above, contact the Main Office Project Liaison (MOPL) (see PDM Exhibit 4-1). Any project which would normally be classified as a CE but could involve unusual circumstances (or even uncertainty) will require consultation with the Office of Environment (OOE) and subsequently with the FHWA to determine if CE classification is still warranted. If, after consultation with the FHWA, it is determined that the project cannot be progressed as a CE, **skip to step 4** and see PDM Chapter 4 for NEPA Class I (EIS) or Class III (EA) processing. If, after consultation with the FHWA, it is determined that the project can be progressed as a CE, **proceed to step 1B**.

If no to all the above, then this project qualifies as a CE; **proceed to step 1B**.

Step 1B: Identification of CE action

Is the project an action listed in 23 CFR 771.117 (c) - (d) (or as identified in [FHWA's additional flexibilities memo](#))?
 YES ☒ NO ☐

If Yes, proceed to step 2.

If No, contact the MOPL (see PDM Exhibit 4-1). If, after consultation with the OOE and the FHWA, it is determined that the project cannot be progressed as a CE, **skip to step 4** and see PDM Chapter 4 for NEPA Class I (EIS) or Class III (EA) processing. If, after consultation with the FHWA, it is determined that the project can continue as a CE, **proceed to step 2**.

¹ See definitions and examples of unusual circumstances in *FEAW_Instructions.doc*

Federal Environmental Approval Worksheet

Project ID Number: 1760.94

Step 2: FHWA environmental actions required prior to CE determination²

The Step 2 table identifies certain issues that require: the FHWA to make the CE determination (Column A and 2.4); independent FHWA determinations (2.1); FHWA approvals, compliance or concurrence (2.2); or notification to the FHWA (2.3). Review *the FEAW Thresholds document* to determine how to fill out each column of Step 2.

2.1	Required FHWA Independent environmental determinations	PARCE threshold exceeded ³	FHWA independent determination/ concurrence required	Date determination/ concurrence issued	Resource not present, or present but threshold not exceeded	
		A	B	B1	C	
	Executive Order (EO) 11990 Protection of Wetlands Individual Finding		<input type="checkbox"/>	Date Issued	<input type="checkbox"/>	
	ESA Section 7 Threatened and Endangered Species	<input type="checkbox"/>	<input type="checkbox"/>	Date Issued	<input checked="" type="checkbox"/>	
	Section 106 of National Historic Preservation Act	<input type="checkbox"/>	<input checked="" type="checkbox"/>	TBD	<input type="checkbox"/>	
	Section 4(f) (Park, Wildlife Refuge, Historic Sites, and National Wild and Scenic Rivers)	<input type="checkbox"/>	<input type="checkbox"/>	Date Issued	<input checked="" type="checkbox"/>	
2.2	Other FHWA environmental approvals, compliance and/or concurrence required	PARCE threshold exceeded ³	Threshold exceeded; FHWA approval, compliance or concurrence required		Resource not present, or present but threshold not exceeded	
	EO 11988 Floodplains	<input type="checkbox"/>	<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	EO 13112 Invasive Species		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	EO 12898 Environmental Justice		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	Safe Drinking Water Act Section 1424(e)		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	US Army Corps of Engineers, Section 404/10 NWP #23		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	Section 6(f) Land and Water Conservation Funds		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	Migratory Bird Treaty Act		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	23CFR772 Type I Noise abatement		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
2.3	Other Environmental Issues requiring FHWA notification	PARCE threshold exceeded ³	FHWA notification threshold exceeded		Resource not present, or present but threshold not exceeded	
	US Army Corps of Engineers, Section 404/10 Individual Permit	<input type="checkbox"/>	<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	National Wild and Scenic Rivers	<input type="checkbox"/>	<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	US Coast Guard Bridge Permit	<input type="checkbox"/>	<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	Known hazardous waste site (only EPA National Priority list)		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
	Project on or affecting Native American Lands		<input type="checkbox"/>		<input checked="" type="checkbox"/>	
2.4	Other Issues Triggering FHWA Approval of Categorical Exclusion	PARCE threshold exceeded ³			Resource not present, or present but threshold not exceeded	
	Property Acquisition	<input type="checkbox"/>			<input checked="" type="checkbox"/>	
	Major Traffic Disruptions	<input type="checkbox"/>			<input checked="" type="checkbox"/>	
	Changes in Access Control	<input type="checkbox"/>			<input checked="" type="checkbox"/>	

² This table does not represent all environmental issues and actions that a project is subject to. Classification as a CE does not exempt the project from further environmental review. Refer to the PDM and The Environmental Manual (TEM) to determine review requirements.

³ When PARCE threshold is exceeded, the NYSDOT recommends that the project qualifies as a CE and requests the FHWA make the CE determination. Information on PARCE specific thresholds are contained within *the FEAW Thresholds document*.

Federal Environmental Approval Worksheet

Project ID Number: 1760.94

Step 3: Who makes the NEPA CE Determination?

To identify which party, either the FHWA or the NYSDOT, makes the CE determination in accordance with the PARCE, follow the instructions found in the table below, beginning in Step 3A. This step also identifies which correspondence shell to use to distribute the FEAW and other environmental notifications or approvals.

3	Determine whether the FHWA or the NYSDOT makes the CE determination and whether additional notifications or approvals are required.
3A	<p>Is the project an action listed in 23 CFR 771.117 (c) - (d) (Answered yes in Step 1B)?</p> <p>YES <input checked="" type="checkbox"/> If Yes, proceed to 3B.</p> <p>NO <input type="checkbox"/> If No, the FHWA makes the CE determination.</p> <ul style="list-style-type: none"> For Locally Administered Federal Aid Projects only, the DAD, the NYSDOT recommendation and request (that the FHWA determines the project qualifies as a CE) are sent from the Regional Planning and Program Manager (RPPM) to the FHWA directly using Shell 4. For all other projects, the DAD and the NYSDOT recommendation and request (that the FHWA determines the project qualifies as a CE) are sent to the MOPL for review using Shell 3. Proceed to Step 4.
3B	<p>Are any of the CE Thresholds from the PARCE not met (Are there any checks in Column A of Step 2)?</p> <p>YES <input type="checkbox"/> If Yes, the FHWA makes the CE determination.</p> <ul style="list-style-type: none"> For Locally Administered Federal Aid Projects only, the DAD and the NYSDOT recommendation and request (that the FHWA determines the project qualifies as a CE) are sent from the RPPM to the FHWA directly using Shell 4. For all other projects, the DAD and the NYSDOT recommendation and request (that the FHWA determines the project qualifies as a CE) are sent to the MOPL for review using Shell 3. Proceed to Step 4. <p>NO <input checked="" type="checkbox"/> If No, proceed to 3C.</p>
3C	<p>Are there outstanding independent environmental approvals or concurrences? (Are there checks in column B of Step 2.1 without dates in column B1)?</p> <p>YES <input type="checkbox"/> If Yes, then the FHWA makes the CE determination.</p> <ul style="list-style-type: none"> For Locally Administered Federal Aid Projects only, the DAD and the NYSDOT recommendation and request (that the FHWA determines the project qualifies as a CE) are sent from the RPPM to the FHWA directly using Shell 4. For all other projects, the DAD and the NYSDOT recommendation and request (that the FHWA determines the project qualifies as a CE) are sent to the MOPL for review using Shell 3. Proceed to Step 4. <p>NO <input checked="" type="checkbox"/> If No, the NYSDOT makes the NEPA CE determination. Proceed to 3D.</p>
3D	<p>Are there</p> <p><input type="checkbox"/> any circumstances requiring demonstration of applicable EO compliance (any checks in column B of Table 2.2); or</p> <p><input type="checkbox"/> any issues requiring the FHWA environmental notification (any checks in column B of Table 2.3)?</p> <p>YES <input type="checkbox"/> If either box is checked, once all required approvals and concurrences have been secured, the NYSDOT makes the CE determination but the information must be forwarded to FHWA for notification or action prior to Design Approval using Shell 1. Proceed to step 4.</p> <p>NO <input checked="" type="checkbox"/> If neither box is checked, once all required approvals and concurrences have been secured the NYSDOT makes the CE determination without notification to the FHWA. The project will use Shell 2. Proceed to step 4.</p>

Federal Environmental Approval Worksheet

Project ID Number: 1760.94

Step 4: Summary and Recommendation

- The project **is not** located within an area subject to transportation air quality conformity.
 - **If the project is within such areas, the NEPA process may not be completed until all transportation conformity requirements are met⁴.** Transportation conformity requirements **have** been met at the time of this signature.
- This project does qualify to be progressed as a Categorical Exclusion.
- The NEPA Determination will be made by NYSDOT
- Project is c(8) "Installation of fencing, signs, pavement markings, small passenger shelters, traffic signals, and railroad warning devices where no substantial land acquisition or traffic disruption will occur." ⁴
- All outstanding FHWA environmental approvals will be obtained and are listed here:
- All the conditions of the PARCE are addressed herein (or within the DAD or attachments).

I certify that the information provided above is true and accurate and recommend the project be processed as described above.

Project Manager/Designer
(or Responsible Local Official)

X

Date _____

Print Name and Title:

Regional Environmental Unit Supervisor

X

Date _____

Print Name and Title:

Regional Local Project Liaison
(Locally Administered Projects Only)

X

Date _____

Print Name and Title:

Changes that may have occurred since the preparation of the FEAW which would create the need to go through the FEAW again include, but are not limited to: a change in the scope of the proposed project; a change in the social, economic or environmental circumstances or the setting of the project study area (i.e. the affected environment); a change in the federal statutory environmental standards: discovering new information not considered in the original process; and a significant amount of time has passed (equal or greater than three years).

⁴ See additional information on identifying (c)26, (c)27 & (c)28 versus d (13) in *FEAW_Instructions.doc*

Appendix C

Traffic Info & Complete Streets Checklist

1. Capital Project Complete Streets Checklist
2. Smart Growth Screening Tool

Chapter 18, Appendix A - CAPITAL PROJECTS COMPLETE STREETS CHECKLIST (18A-4)

PIN:	1760.94	Project Location:	City of Rensselaer
Context:	<input checked="" type="radio"/> Urban / Village <input type="radio"/> Suburban <input type="radio"/> Rural		
Project Title:	Pedestrian Safety Improvements - Washington Ave/Forbes Ave at 7th St, Broadway at Partition St, Broadway at Pine St, Broadway at McNaughton Ave, 1st St/Partition St at East St, and 3rd St at Catherine St.		
STEP 1- APPLICABILITY OF CHECKLIST			
1.1	Is the project located entirely on a facility where bicyclists and pedestrians are prohibited by law and the project does not involve a shared use path or pedestrian/bicycle structure? <i>If no, continue to question 1.2. If yes, <u>stop here</u>.</i>		<input type="radio"/> Yes <input checked="" type="radio"/> No
1.2	a. Is this project a 1R* Maintenance project? <i>If no, continue to question 1.3. If yes, go to part b of this question.</i>		<input type="radio"/> Yes <input checked="" type="radio"/> No
1.2	b. Are there opportunities on the 1R project to improve safety for bicyclists and pedestrians with the following Complete Street features? <ul style="list-style-type: none"> • Sidewalk curb ramps and crosswalks • Shoulder condition and width • Pavement markings • Signing <i>Document opportunities or deficiencies in the IPP and <u>stop here</u>.</i> <small>* Refer to Highway Design Manual (HDM) Chapter 7, Exhibit 7-1 "Resurfacing ADA and Safety Assessment Form" under ADA, Pavement Markings and Shoulder Resurfacing for guidance.</small>		<input type="radio"/> Yes <input checked="" type="radio"/> No
1.3	Is this project a Cyclical Pavement Marking project? <i>If no, continue to question 1.4. If yes, review EI 13-021* and identify opportunities to improve safety for bicyclists and pedestrians with the following Complete Streets features:</i> <ul style="list-style-type: none"> • Travel lane width • Shoulder width • Markings for pedestrians and bicyclists <i>Document opportunities or deficiencies in the IPP and <u>stop here</u>.</i> <small>* EI 13-021, "Requirements and Guidance for Pavement Marking Operations - Required Installation of CARDS and Travel Lane and Shoulder Width Adjustments".</small>		<input type="radio"/> Yes <input checked="" type="radio"/> No
1.4	Is this a Maintenance project (as described in the "Definitions" section of this checklist) and different from 1.2 and 1.3 projects? <i>If no, continue to Step 2. If yes, the Project Development Team should continue to look for opportunities during the Design Approval process to improve existing bicycle and pedestrian facilities within the scope of project. Identify the project type in the space below and <u>stop here</u>.</i>		<input type="radio"/> Yes <input checked="" type="radio"/> No
<div> <div>STEP 1 prepared by:</div> <div>JSG</div> <div>Date:</div> <div>5/21/19</div> </div>			
STEP 2 - IPP LEVEL QUESTIONS (At Initiation)			Comment / Action

Chapter 18, Appendix A - CAPITAL PROJECTS COMPLETE STREETS CHECKLIST (18A-5)

2.1	Are there public policies or approved known development plans (e.g., community Complete Streets policy, Comprehensive Plan, MPO Long Range and/or Bike/Ped plan, Corridor Study, etc.) that call for consideration of pedestrian, bicycle or transit facilities in, or linking to, the project area? <i>Contact municipal planning office, Regional Planning Group and Regional Bicycle/Pedestrian Coordinator.</i>	<input type="radio"/> Yes <input checked="" type="radio"/> No	
2.2	Is there an existing or planned sidewalk, shared use path, bicycle facility, pedestrian-crossing facility or transit stop in the project area?	<input checked="" type="radio"/> Yes <input type="radio"/> No	CDTA stops at various intersections
2.3	a. Is the highway part of an existing or planned State, regional or local bicycle route? <i>If no, proceed to question 2.4. If yes, go to part b of this question.</i> b. Do the existing bicycle accommodations meet the minimum standard guidelines of HDM Chapter 17 or the AASHTO "Guide for the Development of Bicycle Facilities"? * <i>Contact Regional Bicycle/Pedestrian Coordinator</i> <small>* Per HDM Chapter 17- Section 17.4.3, Minimum Standards and Guidelines.</small>	<input type="radio"/> Yes <input checked="" type="radio"/> No <input type="radio"/> Yes <input type="radio"/> No	
2.4	Is the highway considered important to bicycle tourism by the municipality or region?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
2.5	Is the highway affected by special events (e.g., fairs, triathlons, festivals) that might influence bicycle, pedestrian or transit users? <i>Contact Regional Traffic and Safety</i>	<input type="radio"/> Yes <input checked="" type="radio"/> No	
2.6	Are there existing or proposed generators within the project area (<i>refer to the "Guidance" section</i>) that have the potential to generate pedestrian or bicycle traffic or improved transit accommodations? <i>Contact the municipal planning office, Regional Planning Group, and refer to the CAMCI Viewer, described in the "Definitions" section.</i>	<input checked="" type="radio"/> Yes <input type="radio"/> No	Doane Stuart School
2.7	Is the highway an undivided 4 lane section in an urban or suburban setting, with narrow shoulders, no center turn lanes, and existing Annual Average Daily Traffic (AADT) < 15,000 vehicles per day? <i>If yes, consider a road diet evaluation for the scoping/design phase. Refer to the "Definitions" section for more information on road diets.</i>	<input type="radio"/> Yes <input checked="" type="radio"/> No	

Chapter 18, Appendix A - CAPITAL PROJECTS COMPLETE STREETS CHECKLIST (18A-6)

2.8	Is there evidence of pedestrian activity (e.g., a worn path) and no or limited pedestrian infrastructure?	<input checked="" type="radio"/> Yes <input type="radio"/> No	
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STEP 2 prepared by: Date:

Bicycle/Pedestrian Coordinator has been provided an opportunity to comment: ☐ Yes ☒ No

ATTACH TO IPP AND INCLUDE RECOMMENDATIONS FOR SCOPING/DESIGN.

STEP 3 - PROJECT DEVELOPMENT LEVEL QUESTIONS (Scoping/Design Stage)			Comment / Action
3.1	Is there an identified need for bicycle/pedestrian/transit or "way finding" signs that could be incorporated into the project?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.2	Is there history of bicycle or pedestrian crashes in the project area for which improvements have not yet been made?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.3	Are there existing curb ramps, crosswalks, pedestrian traffic signal features, or sidewalks that don't meet ADA standards per HDM Chapter 18 ?	<input checked="" type="radio"/> Yes <input type="radio"/> No	
3.4	Is the posted speed limit is 40 mph or more and the paved shoulder width less than 4' (1.2 m) (6' in the Adirondack or other State Park)? Refer to EI 13-021 .	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.5	Is there a perceived pedestrian safety or access concern that could be addressed by the use of traffic calming tools (e.g., bulb outs, raised pedestrian refuge medians, corner islands, raised crosswalks, mid-block crossings)?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.6	Are there conflicts among vehicles (moving or parked) and bike, pedestrian or transit users which could be addressed by the project?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.7	Are there opportunities (or has the community expressed a desire) for new/improved pedestrian-level lighting, to create a more inviting or safer environment?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.8	Does the community have an existing street furniture program or a desire for street appurtenances (e.g., bike racks, benches)?	<input type="radio"/> Yes <input checked="" type="radio"/> No	

Chapter 18, Appendix A - CAPITAL PROJECTS COMPLETE STREETS CHECKLIST (18A-7)

3.9	Are there gaps in the bike/pedestrian connections between existing/planned generators? <i>Consider locations within and in close proximity of the project area. (Within 0.5 mi (800 m) for pedestrian facilities and within 1.0 mi (1600 m) for bicycle facilities.)</i>	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.10	Are existing transit route facilities (bus stops, shelters, pullouts) inadequate or in inconvenient locations? (e.g., not near crosswalks) <i>Consult with Traffic and Safety and transit operator, as appropriate</i>	<input checked="" type="radio"/> Yes <input type="radio"/> No	
3.11	Are there opportunities to improve vehicle parking patterns or to consolidate driveways, (which would benefit transit, pedestrians and bicyclists) as part of this project?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.12	Is the project on a "local delivery" route and/or do area businesses rely upon truck deliveries that need to be considered in design?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.13	Are there opportunities to include green infrastructure which may help reduce stormwater runoff and/or create a more inviting pedestrian environment?	<input type="radio"/> Yes <input checked="" type="radio"/> No	
3.14	Are there opportunities to improve bicyclist operation through intersections and interchanges such as with the use of bicycle lane width and/or signing?	<input type="radio"/> Yes <input checked="" type="radio"/> No	

STEP 3 prepared by: Date:

Additional comments, supporting documentation and clarifications for answers in step 1, 2 or 3:

PIN# 1760.94

Smart Growth Screening Tool **(STEP 1)**

NYSDOT & Local Sponsors –Fill out the Smart Growth Screening Tool until the directions indicate to **STOP** for the project type under consideration. For all other projects, complete answering the questions. For any questions, refer to Smart Growth [Guidance](#) document.

Title Of Proposed Project:	Pedestrian Safety Improvements at Various Locations
Location of Project:	City of Rensselaer
Brief Description:	Construct pedestrian signals, curb ramps, striping, and other pedestrian improvements at six signalized intersections.

A. Infrastructure:

(Addresses SG Law criterion a. - To advance projects for the use, maintenance or improvement of existing infrastructure)

1. Does this project use, maintain, or improve existing infrastructure?
Yes ☒ **No** ☐ **N/A** ☐

Explain: (use this space to expand on your answers above – the form has no limitations on the length of your narrative)

The City of Rensselaer submitted a PSAP application form with supporting documentation and the following signalized intersections were selected for funding:

1. Intersection of Washington Avenue, Forbes Avenue and 7th Street
2. Intersection of Broadway and Partition Street
3. Intersection of Broadway and Pine Street
4. Intersection of Broadway and McNaughton Avenue
5. Intersection of 1st Street, Partition Street and East Street
6. Intersection of 3rd Street and Catherine Street.

The proposed work will vary per intersection. Prior to construction, all existing curb ramps will be inventoried and evaluated based on the critical design elements for pedestrian facilities. Those not meeting current safety standards will be replaced. Similarly, pavement conditions at crosswalk locations will be reviewed, and crosswalk areas will be milled and resurfaced if pavement is deemed unacceptable. No new utility services are anticipated. but may have to be adjusted if traffic signals are replaced.

A GPS grade field survey will conducted at all intersections to capture curb lines, viisble utilites, and existing striping. Prior to any surveys, DigSafe will be contacted to markout underground utilities.

In general, all crosswalks will be high intensity ladder-style and along with stopbars will be applied using white epoxy reflectorized pavement stripes. Additionally, new pedestrian signals will be installed with Accessible Pedestrian Signals (APS) pushbuttons at each signalized location. Depending upon project funding, the project may replace the two existing traffic signals at Broadway/Pine Street and Broadway/McNaughtont Avenue that do not meet current design standards.

Maintenance Projects:

- a. Continue with screening tool for the four (4) types of maintenance projects listed below, as defined in **NYSDOT PDM Exhibit 7-1 and described in 7-4:**
<https://www.dot.ny.gov/divisions/engineering/design/dqab/pdm>
 - Shoulder rehabilitation and/or repair;
 - Upgrade sign(s) and/or traffic signals;
 - Park & ride lot rehabilitation;
 - 1R projects that include single course surfacing (inlay or overlay), per Chapter 7 of the NYSDOT Highway Design Manual.
- b. For all other maintenance projects, **STOP here**. Attach this document to the programmatic Smart Growth Impact Statement and signed Attestation for Maintenance projects (located in Appendix 2, page 13 in [Guidance](#) document).

B. Sustainability:

NYSDOT defines Sustainability as follows: A sustainable society manages resources in a way that fulfills the community/social, economic and environmental needs of the present without compromising the needs and opportunities of future generations. A transportation system that supports a sustainable society is one that:

- Allows individual and societal transportation needs to be met in a manner consistent with human and ecosystem health and with equity within and between generations.
- Is safe, affordable, and accessible, operates efficiently, offers choice of transport mode, and supports a vibrant economy.
- Protects and preserves the environment by limiting transportation emissions and wastes, minimizes the consumption of resources and enhances the existing environment as practicable.

For more information on the Department's Sustainability strategy, refer to Appendix 1 of the [Guidance](#) and the NYSDOT web site. www.dot.ny.gov/programs/greenlites/sustainability

(Addresses SG Law criterion j : to promote sustainability by strengthening existing and creating new communities which reduce greenhouse gas emissions and do not compromise the needs of future generations, by among other means encouraging broad based public involvement in developing and implementing a community plan and ensuring the governance structure is adequate to sustain and implement.)

1. Will this project promote sustainability by strengthening existing communities?
Yes ☐ No ☒ N/A ☐
2. Will the project reduce greenhouse gas emissions?
Yes ☐ No ☒ N/A ☐

Explain: (use this space to expand on your answers above)

C. Smart Growth Location:

Plans and investments should preserve our communities by promoting its distinct identity through a local vision created by its citizens.

(Addresses SG Law criteria b and c: to advance projects located in municipal centers; to advance projects in developed areas or areas designated for concentrated infill development in a municipally approved comprehensive land use plan, local waterfront revitalization plan and/or brownfield opportunity area plan.)

1. Is this project located in a developed area?
Yes ☒ **No** ☐ **N/A** ☐
2. Is the project located in a municipal center?
Yes ☐ **No** ☒ **N/A** ☐
3. Will this project foster downtown revitalization?
Yes ☐ **No** ☒ **N/A** ☐
4. Is this project located in an area designated for concentrated infill development in a municipally approved comprehensive land use plan, waterfront revitalization plan, or Brownfield Opportunity Area plan?
Yes ☐ **No** ☒ **N/A** ☐

Explain: (use this space to expand on your answers above)

,

D. Mixed Use Compact Development:

Future planning and development should assure the availability of a range of choices in housing and affordability, employment, education transportation and other essential services to encourage a jobs/housing balance and vibrant community-based workforce.

(Addresses SG Law criteria e and i: to foster mixed land uses and compact development, downtown revitalization, brownfield redevelopment, the enhancement of beauty in public spaces, the diversity and affordability of housing in proximity to places of employment, recreation and commercial development and the integration of all income groups; to ensure predictability in building and land use codes.)

1. Will this project foster mixed land uses?
Yes ☐ **No** ☒ **N/A** ☐
2. Will the project foster brownfield redevelopment?
Yes ☐ **No** ☒ **N/A** ☐
3. Will this project foster enhancement of beauty in public spaces?
Yes ☐ **No** ☒ **N/A** ☐
4. Will the project foster a diversity of housing in proximity to places of employment and/or recreation?
Yes ☐ **No** ☒ **N/A** ☐

5. Will the project foster a diversity of housing in proximity to places of commercial development and/or compact development?

Yes ☐ No ☒ N/A ☐

6. Will this project foster integration of all income groups and/or age groups?

Yes ☐ No ☒ N/A ☐

7. Will the project ensure predictability in land use codes?

Yes ☐ No ☒ N/A ☐

8. Will the project ensure predictability in building codes?

Yes ☐ No ☒ N/A ☐

Explain: (use this space to expand on your answers above)

E. Transportation and Access:

NYSDOT recognizes that Smart Growth encourages communities to offer a wide range of transportation options, from walking and biking to transit and automobiles, which increase people's access to jobs, goods, services, and recreation.

(Addresses SG Law criterion f: to provide mobility through transportation choices including improved public transportation and reduced automobile dependency.)

1. Will this project provide public transit?

Yes ☐ No ☒ N/A ☐

2. Will this project enable reduced automobile dependency?

Yes ☐ No ☒ N/A ☐

3. Will this project improve bicycle and pedestrian facilities (such as shoulder widening to provide for on-road bike lanes, lane striping, crosswalks, new or expanded sidewalks or new/improved pedestrian signals)?

Yes ☒ No ☐ N/A ☐

(Note: Question 3 is an expansion on question 2. The recently passed Complete Streets legislation requires that consideration be given to complete street design features in the planning, design, construction, reconstruction and rehabilitation, but not including resurfacing, maintenance, or pavement recycling of such projects.)

Explain: (use this space to expand on your answers above)

This project will replace or construct new pedestrian signals, curb ramps, striping, signing, and other pedestrian improvements to help facilitate crossing at six intersections.

F. Coordinated, Community-Based Planning:

Past experience has shown that early and continuing input in the transportation planning process leads to better decisions and more effective use of limited resources. For information on community based planning efforts, the MPO may be a good resource if the project is located within the MPO planning area.

(Addresses SG Law criteria g and h: to coordinate between state and local government and inter-municipal and regional planning; to participate in community based planning and collaboration.)

1. Has there been participation in community-based planning and collaboration on the project?
 Yes ☐ No ☒ N/A ☐
2. Is the project consistent with local plans?
 Yes ☒ No ☐ N/A ☐
3. Is the project consistent with county, regional, and state plans?
 Yes ☒ No ☐ N/A ☐
4. Has there been coordination between inter-municipal and regional planning on the project?
 Yes ☐ No ☒ N/A ☐

Explain: (use this space to expand on your answers above)

G. Stewardship of Natural and Cultural Resources:

Clean water, clean air and natural open land are essential elements of public health and quality of life for New York State residents, visitors, and future generations. Restoring and protecting natural assets, and open space, promoting energy efficiency, and green building, should be incorporated into all land use and infrastructure planning decisions.

(Addresses SG Law criterion d :To protect, preserve and enhance the State's resources, including agricultural land, forests surface and ground water, air quality, recreation and open space, scenic areas and significant historic and archeological resources.)

Smart Growth Screening Tool (October, 2012)

1. Will the project protect, preserve, and/or enhance agricultural land and/or forests?
Yes ☐ **No** ☒ **N/A** ☐
2. Will the project protect, preserve, and/or enhance surface water and/or groundwater?
Yes ☐ **No** ☒ **N/A** ☐
3. Will the project protect, preserve, and/or enhance air quality?
Yes ☐ **No** ☒ **N/A** ☐
4. Will the project protect, preserve, and/or enhance recreation and/or open space?
Yes ☐ **No** ☒ **N/A** ☐
5. Will the project protect, preserve, and/or enhance scenic areas?
Yes ☐ **No** ☒ **N/A** ☐
6. Will the project protect, preserve, and/or enhance historic and/or archeological resources?
Yes ☐ **No** ☒ **N/A** ☐

Explain: (use this space to expand on your answers above)

Smart Growth Impact Statement **(STEP 2)**

NYSDOT: Complete a Smart Growth Impact Statement (SGIS) below using the information from the Screening Tool.

Local Sponsors: The local sponsors are **not** responsible for completing a Smart Growth Impact Statement. Proceed to Step 3.

Smart Growth Impact Statement

PIN:1760.94

Project Name:Pedestrian Safety Improvements at Various Locations (City of Rensselaer)

Pursuant to ECL Article 6, this project is compliant with the New York State Smart Growth Public Infrastructure Policy Act. This project has been determined to meet the relevant criteria, to the extent practicable, described in ECL Sec. 6-0107. Specifically, the project:

-
-
-
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-

This publically supported infrastructure project complies with the state policy of maximizing the social, economic and environmental benefits from public infrastructure development. The project will not contribute to the unnecessary costs of sprawl development, including environmental degradation, disinvestment in urban and suburban communities, or loss of open space induced by sprawl.

Review & Attestation Instructions (STEP 3)

Local Sponsors: Once the Smart Growth Screening Tool is completed, the next step is to submit the project certification statement (**Section A**) to Responsible Local Official for signature. After signing the document, the completed Screening Tool and Certification statement should be sent to NYSDOT for review as noted below.

NYSDOT: For state-let projects, the Screening Tool and SGIS is forwarded to Regional Director/ RPPM/Main Office Program Director or designee for review, and upon approval, the attestation is signed (**Section B.2**). For locally administered projects, the sponsor's submission and certification statement is reviewed by NYSDOT staff, the appropriate box (**Section B.1**) is checked, and the attestation is signed (Section B.2).

A. CERTIFICATION (LOCAL PROJECT)

***I HEREBY CERTIFY**, to the best of my knowledge, all of the above to be true and correct.*

Preparer of this document:



6/17/19

Signature

Date

Project Engineer

Jeff Gentzler, P.E.

Title

Printed Name

Responsible Local Official (for local projects):

Signature

Date

Title

Printed Name

B. ATTESTATION (NYSDOT)

1. I HEREBY:

☐ *Concur with the above certification, thereby attesting that this project is in compliance with the State Smart Growth Public Infrastructure Policy Act*

☐ *Concur with the above certification, with the following conditions (information requests, confirming studies, project modifications, etc.):*

(Attach additional sheets as needed)

☐ *do not concur with the above certification, thereby deeming this project ineligible to be a recipient of State funding or a subrecipient of Federal funding in accordance with the State Smart Growth Public Infrastructure Policy Act.*

2. NOW THEREFORE, pursuant to ECL Article 6, this project is compliant with the New York State Smart Growth Public Infrastructure Policy Act, to the extent practicable, as described in the attached Smart Growth Impact Statement.

NYSDOT Commissioner, Regional Director, MO Program Director,
Regional Planning & Programming Manager (or official designee):

Signature

Date

Title

Printed Name